

SETBACKS

NOT COMPLETE
ON SETBACKS
Some Commercial
MISSING

CENTRAL

ARTCRAFT

1.1 Setbacks

Objective: To provide a relatively continuous and homogeneous building fabric appropriate for a commercial street.

Commentary: Under the ICO the Department of Building and Safety, perhaps unaccustomed to dealing with mixed use zones, is currently applying required residential yard standards to those floors where there will be a residential use, leaving a permitted building envelope somewhat like a tiered wedding cake where commercial use on the ground floor supports residential above. The mixed use is appropriate to the area and Venice's historical role as home to many artists. The buildings generated by this mixed use zoning must, however, be appropriate to the commercial look of the street.

It should be noted that there is a conflict in the ICO regarding setbacks. Under Area 8 a minimum five foot front yard setback is called for, which overrides the requirements of commercial zoning falling within that area where 0 to five foot *maximum* front yard setback is specified.

Guideline: The following minimum setbacks shall be allowed for all uses in the Artcraft Zone:

Front Yard Setback	0'
Rear Yard Setback	0'
Side Yard Setback	0'

RESIDENTIAL

7.3 Setbacks

7.3.1 Front Yard Setback

Objective: To produce a street edge compatible with the existing fabric. To encourage open porches on new construction.

Commentary: Porches are a very important element in Venice housing, due to the small lots in this area. Not only do they provide a transition zone and sense of entry, they encourage neighborhood interaction while creating visual interest from the street. Suitable area for landscaping should be provided to soften the street edge at the pedestrian level.

Guideline: The front yard setback shall be twelve feet.

Covered, open porches may intrude into the required front yard setback up to six feet. Where open porches are provided, enclosed living space on the lowest floor may intrude up to six feet into the required front yard setback, provided it does not exceed 60% of the length of the building's front facade. Living space at the second story shall have a required front yard setback of twelve feet, six feet when open porches are provided at the first story. The third story shall have a required front yard setback of twelve feet, regardless of whether porches are provided.

In exceptional cases, an average of front yard setbacks over the entire block can be used to establish minimum front yard setbacks less than those prescribed above.

Alternative A:

**Commentary:* In agreeing with the main suggestion that open porches should be encouraged, we nevertheless feel that twelve feet is too much. It will force porches on all new houses uniformly without allowing for individual taste, in order to take advantage of the much larger possible floor area (extra six feet) allowed if a porch is built.

Instead of twelve feet, a nine foot setback would be more appropriate and would allow for more individual choice relative to this feature.

Twelve feet is simply too much to require on small lots.

**Guideline:* The front yard setback shall be nine feet.

Covered, open porches may intrude into the required front yard setback three feet. Where open porches are provided, enclosed living space may intrude three feet into the required front yard setback, provided it does not exceed 60% of the length of the building's front facade on the lowest floor. Living space at upper floors have a required front yard setback of nine feet, six feet when open porches are provided at the first story.

No averaging of front yard setbacks shall be allowed.

Alternative B:

**Commentary:* Many people like having large, private back yards. If twelve foot front yard setbacks are required, we eliminate the choice of a decent size backyard. The current guidelines under the ICO allow more flexibility.

**Guideline:* The front yard setback shall be five feet.

7.3.2 Rear Yard Setback

Objective: To create fire breaks and allow Fire Department access. To allow access to parking from the alley.

Commentary: Existing conditions are extremely varied in Venice, including many garages and living units built to the rear property line. While current parking requirements require a 22'-6" sweep into a parking space, the reality is that many Venice residents maneuver full size cars into parking spaces and garages located directly on the property line abutting 15' alleys. It is not believed that this is either dangerous, given the traffic loads of our alleys, or a hardship to present or future residents given the potential benefit of increased *usable* open space that can be derived on these small lots. Density limitation and higher construction costs incurred by building over required parking will prevent all buildings from taking advantage of this guideline. Reduced illegal alley parking (a common nuisance) is anticipated.

Guideline: A rear yard setback of 7'-6" is required from the centerline of the alley. On through-block parcels, front yard setbacks shall be applied. On lots with no alleys which are not through-block, side yard setbacks shall be applied.

Alternative A:

***Guideline:** The main outline is accepted and in addition it is *emphastzed* that no setback from the property line is required. If such

setback does occur on the ground floor, it shall be allowed to cantilever the second and third story over the parking area to the property line.

7.3.3 Side Yard Setback

Objective: To provide adequate fire break, light and air. To encourage architectural projections and a heightened level of visual interest.

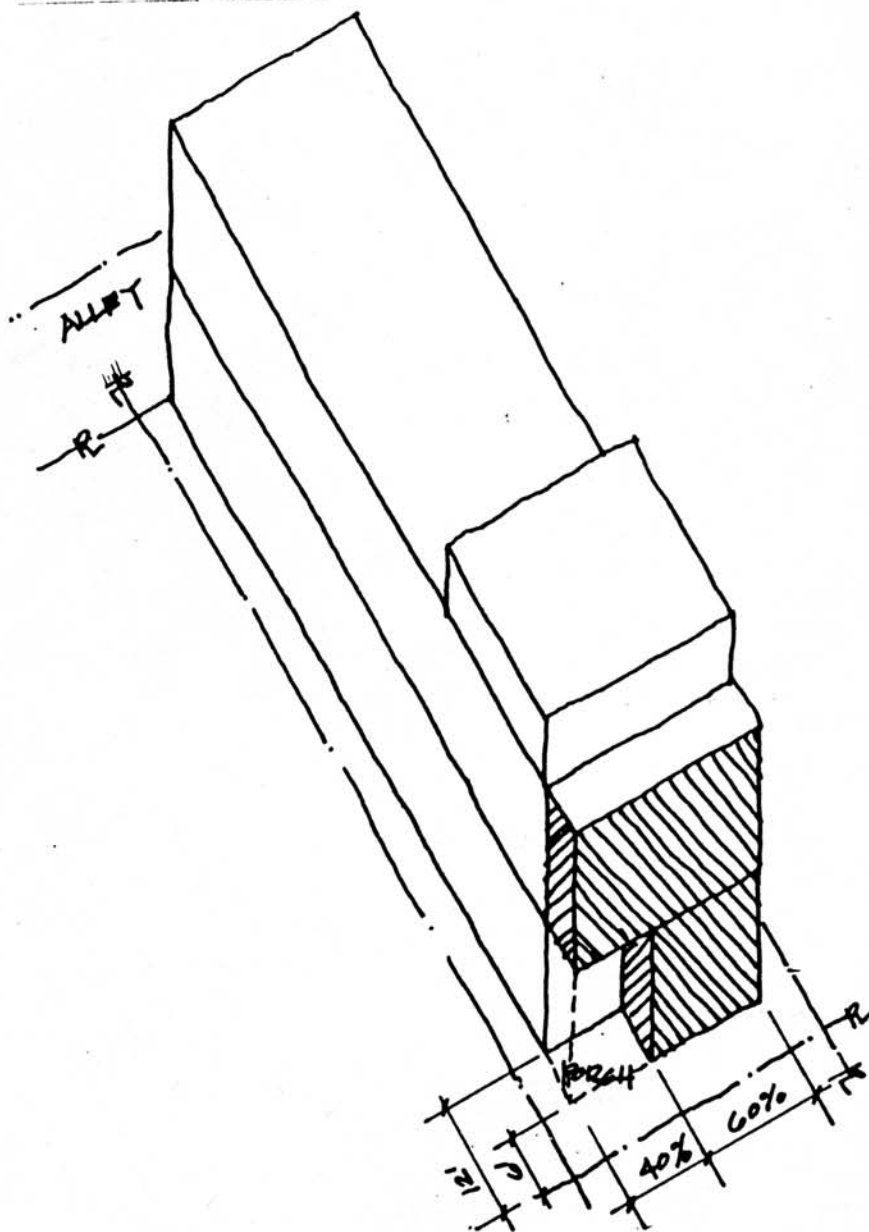
Commentary: Current codes do not permit architectural projections within thirty inches of the property line. Current building practice is to build to the three foot setback and ignore the additional six inches offered by the architectural projections. A more generous required setback, it is believed, would encourage use of architectural projections, providing heightened articulation and visual interest while permitting increased light and air.

Guideline: A five foot side yard is required. On lots less than 50 feet in width, 10% of the lot width may be used. In no case shall the required side yard be less than four feet. Subterranean parking may encroach into the side yard to the property line provided that it is fully depressed, with the roof being at or below natural grade.

Alternative A:

See Rovins' opinion.

Porch Incentive Diagram



Alternative B:

***Commentary:** A four foot setback is too much considering the small width of many lots in Venice.

The attempt to encourage architectural projections leaves room for abuse.

***Guideline:** A three foot setback shall be required.

Alternative C:

***Guideline:** Subterranean parking may encroach to the property line, provided that the roof of the subterranean parking shall be no more than 36" above natural grade.

N. VENICE AREAS B + C.

WALK STREETS

5. Front Yard Setback.

Objective: To produce a street edge compatible with the existing fabric. To encourage porches on new construction.

Commentary. The walk streets are unique in that many of the houses extend to the very edge of their property line. Technically, the city owns the bulk of most people's gardens, making the questions of front yard setbacks often an academic one. (This problem would be alleviated if the city in good faith gave these front yards to the property-owners who have maintained them for decades.) Our commitment is to preserve a contiguous line on each walk street. Porches should be encouraged by trade-offs with setbacks.

Guidelines: The front setback of new construction should in no case extend in front of another house. The front-line, including the porch, should be contiguous with other houses on the walk street.

Open porches may intrude in the required front yard setback up to eight feet, if they do not extend beyond the front of the neighboring houses. An open porch is defined by open-wall area of 50% or more. Where open porches are provided, enclosed living space may intrude up to eight feet into the required front yard setback, provided it does not exceed 50% of the length of the building's front facade on the lowest floor. The enclosed area must maintain the 50% open area with the enclosure only in clear glass.

If a porch is provided, upper floors can extend over porch.

6. Rear Yard Setback

Objective: To create fire breaks and allow Fire Department access. To allow access to parking from the alley, including a turning ratio.

Guideline: Rear yard setbacks should be maintained. A 15' rear yard is required. One-half of the width of the alley may be used in computing the depth of the rear yard.

7. Side Yard Setback

Objective To provide adequate fire breaks, light and air. To encourage architectural projections and provide visual interest.

Commentary: In addition to encouraging light and air, we would also like to provide an incentive for architectural protrusions such as chimney, bay windows, and eaves.

Guidelines: The standard setback will be 10% of lot width, with a 4' minimum. The setback can be reduced to 30" (Fire Department minimum clearance) for the 25% of the side wall area consisting of bay windows or other variations. (36" clear minimum for first eight feet)

Protrusions cannot extend more than 2 1/2 feet from building.

Any portions of the side wall that exceed 20', not including gables, must be set back one foot over the allotted side yard setback.

DRIVE STREETS

7.3 SETBACKS

7.3.1 Front Yard Setback

Objective: To enhance the pedestrian experience, while facilitating building on substandard size lots. To encourage porches.

Commentary: Porches are a characteristic feature of the historical Venice beach community. They provide a desirable street friendliness and encourage neighborhood interaction. In so doing, they also enhance neighborhood security, which is a major need on these high use drive-thru beach streets west of Pacific.

Guideline: The front yard setback shall be ten feet. Open porches may intrude into the required setback 5 feet. Where open porches are provided, enclosed living space above may intrude up to five feet into the required front yard setback provided it does not exceed 65% of the length of the building's front facade on the lowest floor. Living space at upper floors have a 10 foot setback, 5 foot when porches are provided at the first story.

2 Rear Yard Setback

Objective: To maximize open space capability in construction on a substandard lot.

Commentary: Many garages and buildings are already built to the alley line on these streets. It is reasonable to allow future buildings the same setback. An alternative would be to allow an average of adjacent property setbacks.

Guideline: A rear yard setback of 7'6" is required from the centerline of the alley.

7.3.3 Side Yard Setback

Objective: To provide adequate light, air and firebreak. To promote articulation of sideyard elevations.

Commentary: We want to develop an incentive for sideyard articulation. Older Venice buildings have bay windows, large roof eaves, and chimneys creating visual interest in their sideyards. Currently, most new buildings have flat-side elevations. We would like to encourage the development of form by increasing sideyard requirements to 4 feet, while at the same time allowing a one and one half foot encroachment into that sideyard by architectural projections. This provides a carrot to the builder in allowing a bigger floor area only if articulation of this sort is developed.

Guideline: Sideyard setback shall be 4 feet. Architectural projections including cornices, sills, chimneys, bay windows, and vertical projections may encroach upon this setback for up to 60% of lot length. Roof eaves may project the length of the building. Fire code requirements of 30" setback shall be maintained in all cases.

COMMERCIAL

Front Setback

Objective: To promote vending that is enclosed in a building.

Commentary: A required front setback for commercial property would be of little value to the public.

Guideline: A 0-foot front setback shall be required for commercial buildings.

Rear Setback

Objective: To allow a reasonable space for the delivery of commercial goods and the enclosed storage of waste.

Commentary: Requiring a rear setback in addition to the requirements on commercial property for loading docks and enclosed trash storage is unreasonable.

Guideline: A 0-foot rear setback shall be required for commercial buildings.

Side Setback

Objective: To provide adequate light and air between buildings.

Commentary: The Coastal Guidelines require a 6-foot separation between buildings for every 50 feet of frontage. It is of extremely high value in the Coastal Zone to avoid new construction of massive buildings that consolidate lots; however, side yards at ground level on a commercial street in Venice create spaces where people loiter, urinate, etc.

Guideline: Commercial side setback shall be 0 feet at ground level; however, the side setback shall be 5 feet or 10% of the width of the lot with a 3-foot minimum on each side of the building above 15 feet or the top of the first storey, whichever is lower.

1.3 Setbacks

Residential Properties (Washington to Venice)

- 1.3.1 These should be similar to walk streets with some provisions
- 1.3.2 for higher fences because of the much greater numbers of
- 1.3.3 people walking by these properties so that these owners may retain more privacy

Commercial Properties (Venice to Breeze)

1.3.4 Front Yard Setback

Objective: To produce a street edge compatible with the existing fabric. To discourage the congregation of passers by or places where people may loiter or set up vending areas.

Commentary: Ocean Front Walk is a very high foot traffic area; in the commercial zone people tend to fill all available areas. The providing of a setback from the front property line of a building, in this area, will effectively take that piece of property away from the lot and will serve no useful purpose. As this is on the ocean front there is wide open space to the west.

Guideline: No front yard setback commercial zone.

1.3.5 Rear Yard Setback

Objective: To allow delivery of commercial goods, to allow the clean storage of waste from businesses, to allow access to parking.

Commentary: Because of the complexities of operating the various businesses; and the requirements of such things as loading docks, higher parking requirements than residential zones, greater building code requirements owners of these lots must be given maximum use of their substantially undersized lots to provide all of the code requirements that were developed with 5000 square foot lots in mind.

Guideline: No rear yard setback.

1.3.6 Side Yard Set Back

Objective: To provide adequate light and air between buildings. To encourage architectural projections and a heightened level of visual interest.

Commentary: Current code allows commercially zoned properties to build to the property line. In an ocean front area this can lead to many buildings having walls that touch and creating a "row house" effect blocking out light and air and a feeling of spaciousness that should exist on the ocean front. Side yards on ground level in this area are problematic that they create spaces where people urinate and defecate, spaces where people drink, do drugs, and sleep overnight. Side yards also become garbage collectors with trash blowing in from the Ocean Front Walk with the coastal winds. A compromise effect needs to exist on Ocean Front Walk.

Guideline: On the first level commercial projects may build to the property line. On upper levels they are to have setback that equal 10 per cent of the lot width on either side.

COMBINED AREA WIDE DEVELOPMENT STANDARDS

RESIDENTIAL

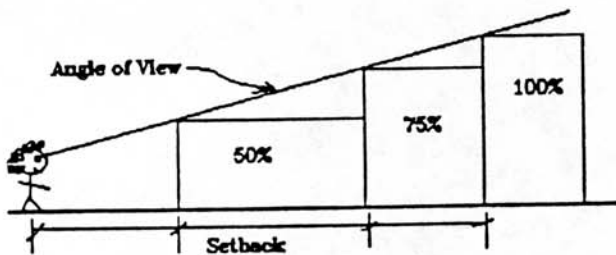
7.3.1 Front Yard Setback

I feel that the current front yard setback of 15' on all new buildings is adequate and provides plenty of open space in the front yard area. Also the current side & rear yard setbacks are adequate.

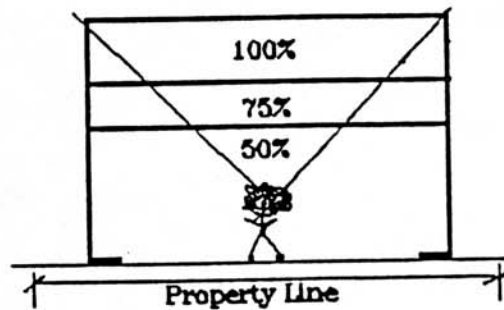
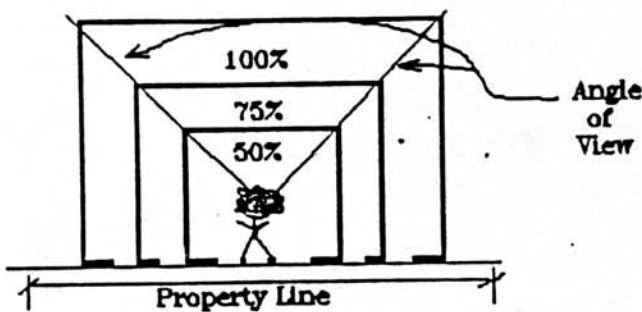
Objective: To maintain the present feeling and diversity of the buildings in Venice without overly regimenting the ideas and creations of residents or developers.

Commentary: Several terminologies and conceptual approaches have been discussed during these design workshops which are intended to guide or mandate the development of new structures within the area. However, if one were to take a worst case scenario of what could be built under the guidelines of several of the proposed standards, and compare those with the objectives of the same proposals, one would find areas of conflict within the same document. This exists because the limiting factors used to control the structures' mass do not address a key issue, which is what a person looking at a building will actually see and be aware of.

Guideline: The solution to this, then, is to use an alternate approach which incorporates the visual aspects. In order to do this, the height, setbacks and volume of a building must be looked at as one entity and considered within a specified line of sight. To put this another way, the maximum height and mass of a building must be related to its distance from the passer-by, according to a certain angle of view. This concept can be more clearly described with a visual image (see below).



Note: The percentage of mass may be moved around within the angle of view to produce architectural variations (e.g., porches, bay windows, second or third story setback, etc.)



I. Building Mass

The suggested guideline values for Angle of View (AOV), Building Volume and Setback are maximum relative numbers, not absolutes. This means that if someone wants to decrease the AOV by 10%, then the building volume may be increased by an equal percentage of the allowed amount or the setback may be decreased by an equal percentage amount. The important issue when determining the envelope of a structure is to keep in mind the overall mass within the angle of view.

Note: The AOV starts 4'-6" above the ground level (the height of the average person's eyes) for measuring front and side setbacks, and 14'- 6" (above the first story of garage) when measuring rear setbacks. Anyone reviewing this document may wish to also review the more than 85 sightings that were made in preparation for this document. All the sightings were measured for angle of view and front yard setback, and a photo of the structure was taken. The buildings were chosen randomly with the idea of sighting all types of structures. To review the sightings, you may contact James Murez at 213-216-9516 M-F 9-5.

Walk Streets:

Point of View: Center of public right of way

Maximum Values

FAR 1.5

Height Not To Exceed 32 Feet

Building Volume:

50 Percent at 12 Feet Front Setback with a 30 Degree AOV						
75 "	"	18 "	"	"	"	"
100 "	"	24 "	"	"	"	"
50 "	"	2.5 "	Side "	"	"	86 "
100 "	"	4 "	"	"	"	"
100 "	"	0 "	Rear "	"	"	86 "

Drive Streets (residential):

Point of View: Opposite side of street from center of sidewalk

Maximum Values

FAR 1.5

Height Not To Exceed 40 Feet

Building Volume:

50 Percent at 5 Feet Front Setback with a 30 Degree AOV						
75 "	"	8 "	"	"	"	"
100 "	"	10 "	"	"	"	"
50 "	"	3 "	Side "	"	"	86 "
100 "	"	4 "	"	"	"	"
100 "	"	0 "	Rear "	"	"	86 "

Drive Streets (commercial):

Point of View: Opposite side of street from center of sidewalk

Maximum Values

FAR 1.5

Height Not To Exceed 55 Feet

Building Volume:

75 Percent at 0 Feet Front Setback with a 30 Degree AOV						
100 "	"	5 "	"	"	"	"
100 "	"	0 "	Side "	"	"	90 "
100 "	"	0 "	Rear "	"	"	90 "

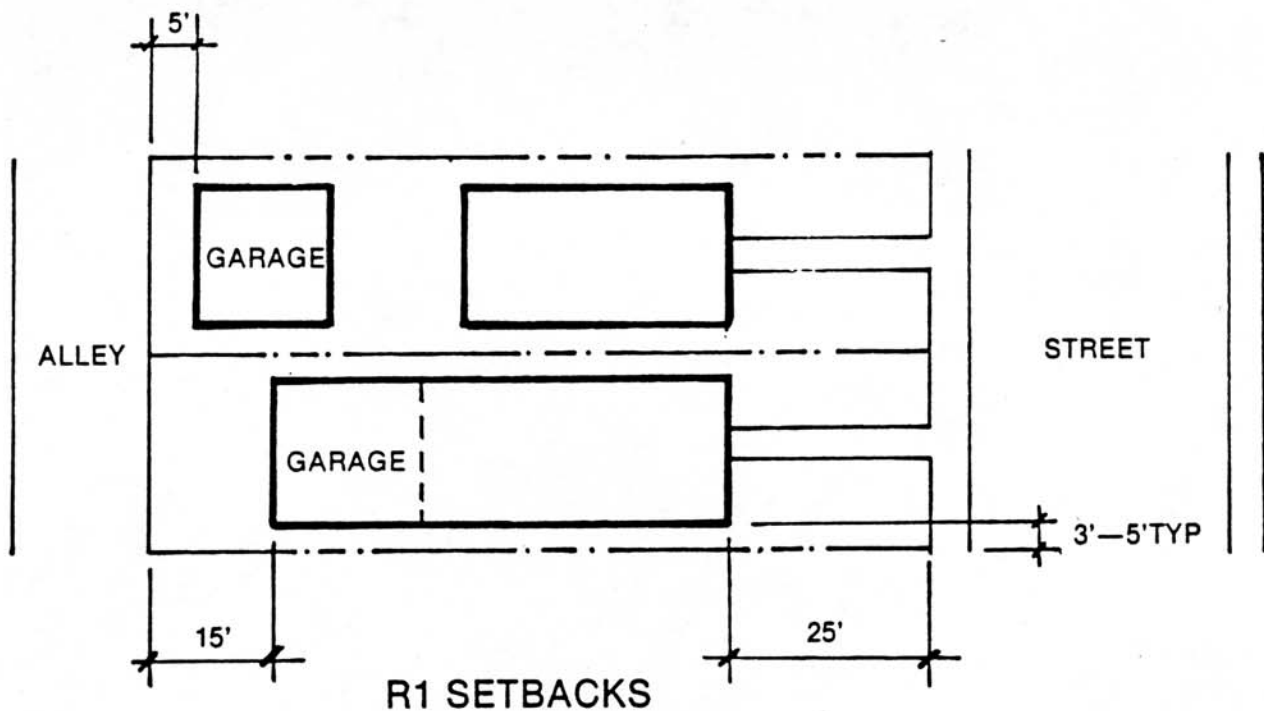
In all cases where two types of areas (i.e., walk streets, residential drive streets, and commercial drive streets) abut each other, the AOV restrictions of the lesser shall apply. An example of this would be where a house on a walk street abuts to the rear of a commercial building on a drive street. The AOV for the rear of the commercial building is 86 degrees starting at 14'-6" above ground, but its maximum height cannot extend into the residential front AOV of 30 degrees.

OXFORD TRIANGLE

RESIDENTIAL R. 1

Front Setbacks: The average of existing setbacks on the entire block. Establish a minimum of twenty five feet on any new development.

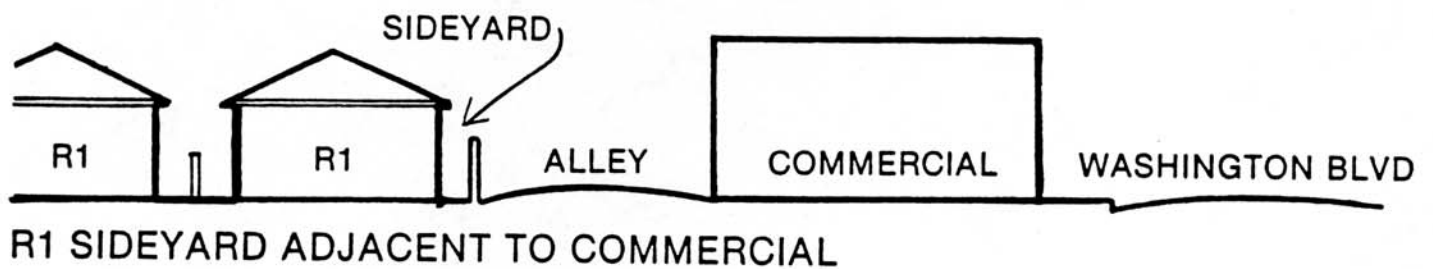
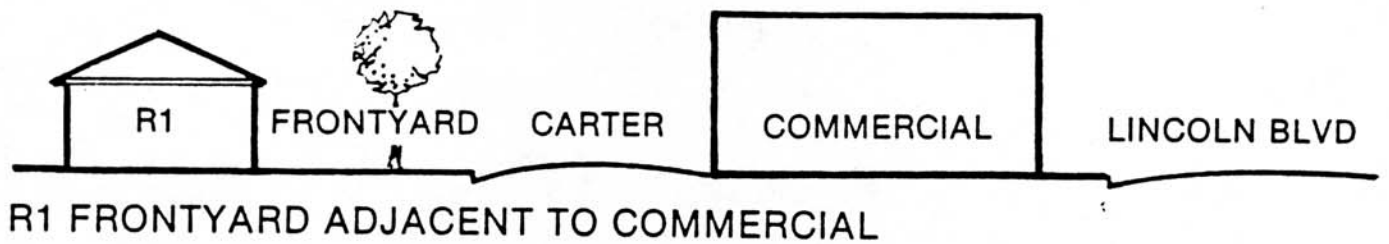
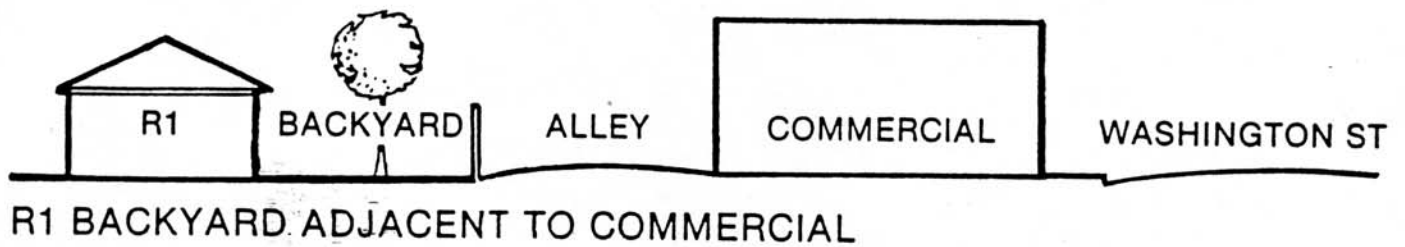
Side Setbacks: 10% of lot width with a minimum of three feet for lots under thirty feet wide. A provision should be made allowing older houses on substandard lots to remodel without having to conform to these standards unless they tear down all but one wall. In the case of nonconforming existing setbacks, only safety should be considered for requiring a new setback for a property.



Rear Setbacks: Option A- Fifteen feet for an attached garage. Five feet for detached garage. Option B- Increase from fifteen feet to eighteen feet to allow for adequate off street parking.

Setbacks for Irregular or Odd Shaped Lots: Setbacks should be considered on an individual basis. In instances where a variance is needed to build or improve on an irregular lot, the owner should seek approval from 2/3rds of the property owners surrounding the site.

Reasons: To provide physical and visual open space. Create spaces for light and air flow. Provide sufficient fire breaks and fire department access.



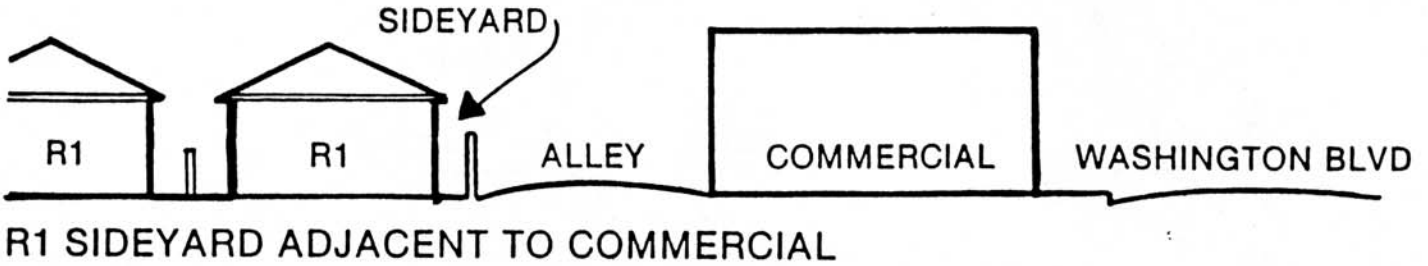
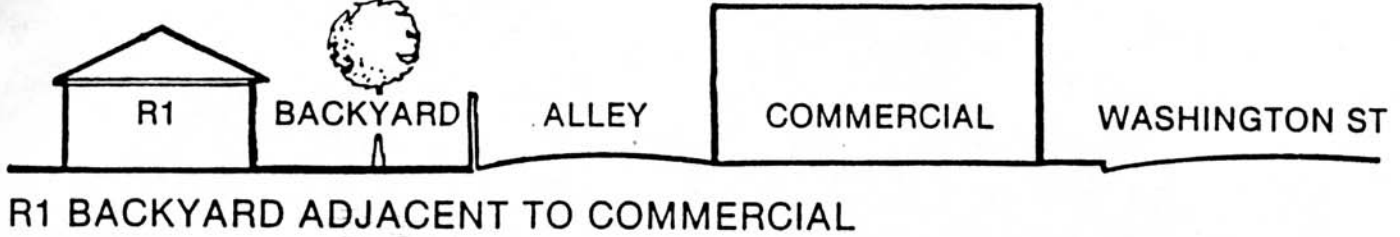
COMMERCIAL ON WASHINGTON BLVD./ST (AND LINCOLN BLVD)

Our main concern is that the existing residential is properly protected from the existing and any future commercial development on Washington. A clean quiet and serene alley between the R1 and C2 zones should be established to act as a buffer between the noisy commercial activities and the quiet residential neighborhood.

Adjacent single family residences should be buffered and isolated from commercial uses including: parking lots, sources of noise, lighting, odor, dumpsters service vehicles and other annoyances inherent to commercially zoned projects and incompatible with existing residential homes.

Rear setbacks for parking or other uses should be discouraged at the rear of the commercial (See item 5). A ten foot high reinforced, split faced masonry block wall should be built along the alley property line at all locations not occupied by a building.

Reasons: The alleys are the only means by which to buffer the residential from the commercial and they are inadequate. We feel that these requirements should be made and strictly adhered to.



DAKWOOD

ISSUE: RESIDENTIAL SETBACKS

OBJECTIVE: Maintain the character of Oakwood, with space between buildings in line with the existing small scale look and feel.

COMMENTARY:

Useful open spaces, solar access, and architecturally interesting projects should be promoted.

GUIDELINES:

1. Require a front setback of at least 15 feet from the front property line (front property line is generally understood to be the inside edge of the sidewalk).
2. Require each side setback to be not less than 3 feet for one-story construction and 5 feet for two-story construction (the first floor could have 3 foot setback, with the second floor stepped in to 5 feet from the property line).
3. Require a rear setback of at least 15 feet from the center line of the alley.

LINCOLN BLVD. COMMERCIAL

3. Consider granting a height increase in exchange for setback, landscaping and affordable housing.

VENICE CANALS

3. SETBACKS

Current Requirements:

CODE - Front setback - 10'; Side setback - 10% of lot width, minimum 3' for one-story and two-story buildings, increased at the rate of 1' for each additional story; Rear setback - 15' from center line of court (alley) with 9' rear setback from property line at first story, second story balcony can extend 4' into the rear setback. ICO and COASTAL - Same as code but requires 450 square feet of permeable yard area between the canal property line and the front of any structure, (ICO only: must be open and unobstructed to the sky). No fill shall be permitted in this yard area. No project shall be permitted in this required yard area with the exception of permeable decks at grade and fences which do not exceed 42" in height. In no case shall this yard be less than 10" wide.

Objective:

To ensure the use of 4' side setbacks for three story buildings.

Commentary:

The participants agree that the 4' side yard requirements for three story buildings must be enforced to bring side yard requirements in compliance with other residential zones. (Refer to Section 4 - BASEMENTS as it relates to setbacks.)

Additionally, in the case of detached garages, all but one member agreed that all structures maintain the above minimum setbacks. No zero lot line structures should be allowed.

Recommendations:

1. Require compliance with the definition of "story" as defined in the zoning ordinance for side yard setbacks. (The zoning code requires 4' side setbacks for 3 story structures.)
2. Setbacks remain per code for front and rear.
3. All structures, including detached garages, shall maintain the minimum 3' side setback.
4. Allow the use of permeable decks over the entire 450 square feet of required permeable front yard provided such deck does not exceed a height of 2' above natural grade and the fence does not exceed 42" above natural grade.

N. VENICE AREA A.

WALK STREETS

C. Set Backs

1. Front Yard

15' or the average stringline, whichever is more lenient. All buildings shall have their main entrance located for access from the front property line adjoining the walk street.

Discussion took place about the encroachments into the public rights-of-way. It was decided that either the encroachments should be stopped and the parcels returned to the City, or the owners should buy the parcels and pay taxes on them. The money collected for the purchase of the land should be recycled back into the Venice community.

2. Side Yard

Use the current L.A. Building Code guidelines.

3. Rear Yard

Use the current L.A. Building code guidelines.

RESIDENTIAL DRIVE STREETS

B. Set Backs

1. Front Yard

15' or the average stringline, whichever is more lenient.

2. Side Yard

Use the current L.A. Building Code guidelines.

3. Rear Yard

Use the current L.A. Building Code guidelines.

OCEAN FRONT WALK

C. Set Backs

1. Front Yard

Comply with the one foot setback ordinance for commercial usage. 15' or the average stringline, whichever is more lenient, for the residential portion.

2. Side Yard

Zero set back for the commercial space. Use the current L.A. Building Code guidelines for the residential space.

3. Rear Yard

Use the current L.A. Building Code guidelines.

NORTH VENICE AREA A "PRESERVING THE FUTURE"

V. RESIDENTIAL SETBACKS

A. FRONT SETBACK

1. DEFINITION

The distance from the front property line to any structure of the building that faces the front property line.

2. ICO

(ICO Sec 563 Pg 30)

Not less than 5 feet .

Additional Municipal Requirements such as R3 requires 15 feet.

3. CURRENT CONDITIONS

Various conditions currently exist with setbacks from 5 feet to 20 feet depending on the property.

4. ISSUES

- Should different streets have different setback limits?
- Should different uses have different setback limits?
- Should exceptions be allowed to the setback limits?
- Should exceptions be allowed to promote a particular architectural form such as porches.
- How should balconies or other encroachments such as window boxes and decoration be calculated?

5. OBJECTIVES

To preserve the present character and scale of the neighborhood. To limit the scale of new development that may contribute to further impaction of the area. To keep the front setbacks of new developments consistent with the majority of existing development.

6. RECOMMENDATION ON FRONT SETBACKS

The Municipal Code requirements for RD1.5 zones of 15 feet front yard minimum setback should be followed on all residential units. In addition to the size of front setbacks at least 25% of the front setback area should be landscaped with permeable material in order to soften the relationship with adjoining areas and provide moisture absorption.

7. JUSTIFICATION

The RD zoning requirement of 15' front yard setbacks is consistent with the majority of existing residential development.

8. REAR SETBACK

1. DEFINITION

The distance from the rear property line to any structure of the building that faces the rear property line.

2. ICD

No provision in the ICD Municipal Requirements require a 15' setback of which up to half can be measured from the center line of alley. Projections are permitted up to 4 feet.

3. CURRENT CONDITIONS

Various conditions currently exist with setbacks from 0 feet to 20 feet depending on the property.

4. ISSUES

- Should different streets have different setback limits?
- Should different uses have different setback limits?
- Should exceptions be allowed to the setback limits?
- Should exceptions be allowed to promote a particular architectural form such as balconies.
- How should balconies or other encroachments such as window boxes and decoration be calculated?
- How should parking structures be calculated?

5. OBJECTIVES

To preserve the present character and scale of the neighborhood. To encourage diversity of design. To maintain free access through alleyways. To assure adequate fire department access.

6. RECOMMENDATION ON REAR SETBACKS

Utilize Municipal Requirements which provide for a 15' setback of which up to half can be measured from the center line of alley. Projections are permitted up to 4 feet for balconies, open stairs and decoration. Parking spaces may extend to the property line provided there is a minimum of 22'6" to turn into the parking space.

Additional limitations, however, should be placed on the overall size of any projections to assure they are of reasonable overall scale.

7. JUSTIFICATION

Alleyways are an essential part of local traffic movement for the purpose of fire department access, service access, and parking access. During heavy visitor periods traffic in the alleyways is further intensified. The 15' set back requirement assures that there is adequate room for the free flow of traffic and adequate building separation in the event of fire.

C. SIDE SETBACK

1. DEFINITION

The distance from the side property line to any structure of the building that faces the side property line.

2. ICO

No provision in the ICO. Municipal Codes requires a minimum of 5 feet. For buildings of 50' width. For buildings 50' the requirement is 10% of the building width but not less the 3 feet as the side setback. At least 6 feet between buildings. Architectural projections are allowed within 30" of the property line.

3. CURRENT CONDITIONS

Various conditions currently exist.

4. ISSUES

- Should the current setback be increased to provide additional light and air?
- What provisions if any should be made for architectural projections or window boxes.
- How should design variation be encouraged?

5. OBJECTIVES

- To preserve the present character and scale of the neighborhood.
- To encourage diversity of design.
- To maintain adequate light and ventilation for adjoining properties.
- To assure adequate fire separation and access for fire fighting between buildings.

6. RECOMMENDATION ON SIDE SETBACKS

Utilize Municipal Codes which provide for a minimum 3' side setbacks, or 10% of the building width, whichever is greater. For each story above two there is to be one additional foot of side set back. In no event shall buildings be closer than 6 feet to the adjoining building, and in no event should projections of any kind go beyond the 3 feet.

MILLWOOD

RESIDENTIAL

Setbacks -- R2

OBJECTIVE: To maintain as many open and useful spaces as possible, consistent with Los Angeles City emergency access requirements.

To promote architecturally interesting projects.

COMMENTARY: Regarding rear yard setback, the group felt that existing restrictions may be excessive. We have no objection to the existing conditions where buildings are close to alleys.

- GUIDELINES:
1. Front Yard Setbacks and Side Yark Setback: No change from present city codes and the ICO.
 2. Rear Yard Setbacks: 15 ft. from center of alley. (Guideline derived from RD1.5 limits)

COMMERCIAL

Setbacks -- Lincoln Boulevard

Five feet of side yard setback will be required at the entrances to the walk streets off of Lincoln Blvd.

Setbacks -- West Washington Boulevard

1. Sideyards - There will be sideyard setbacks of five feet or 10% of lot width, whichever is less, where the interior side parcel line abuts a residential area.
2. Frontyards - as per the ICO.
3. Rearyards - Where rear parcel lines abut a residential use, setback will be the same as for residential lots.

PHILOSOPHY

When different zones are neighboring, we want the higher zoning classification property to provide a larger setback buffer.

FRONT

BACK

SIDES

R-1	Prevailing setback, or 20%, whichever is <u>less</u> . (Porches can encroach.)	With alley: 15' from centerline Without alley: 15' from property line	10%, minimum 3 ft.
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Regarding placement of garage at back of lot..turning radius is an issue, we may want small setb'k.

R-2			
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R-3	15'	with R3/R4 neighboring and with or without an alley: prevailing setback.	with R3/R4 neighboring, 10%, min. 5'
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R-4		with R1/R2 neighboring, and no alley: 15' w/ alley: 20' from centerline	with R1/R2 neighboring, 10%, min. 10 ft.
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C-1
C-2
C-4

We did not work out numbers for commercial or industrial zoning setbacks. Our philosophy is that we want a landscape buffer setback at whatever property line abuts a different zone, or is across a street or alley from a different zone. Parking lots must have landscape buffer strip. We would consider having new construction allowed on an alley property line if the building were articulated to provide a planting area, as an alternative to a full buffer setback.

H-1			
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DISSENT: One person wishes to have no setback required for R-1/R-2 back property line.