

III

OCEAN FRONT WALK

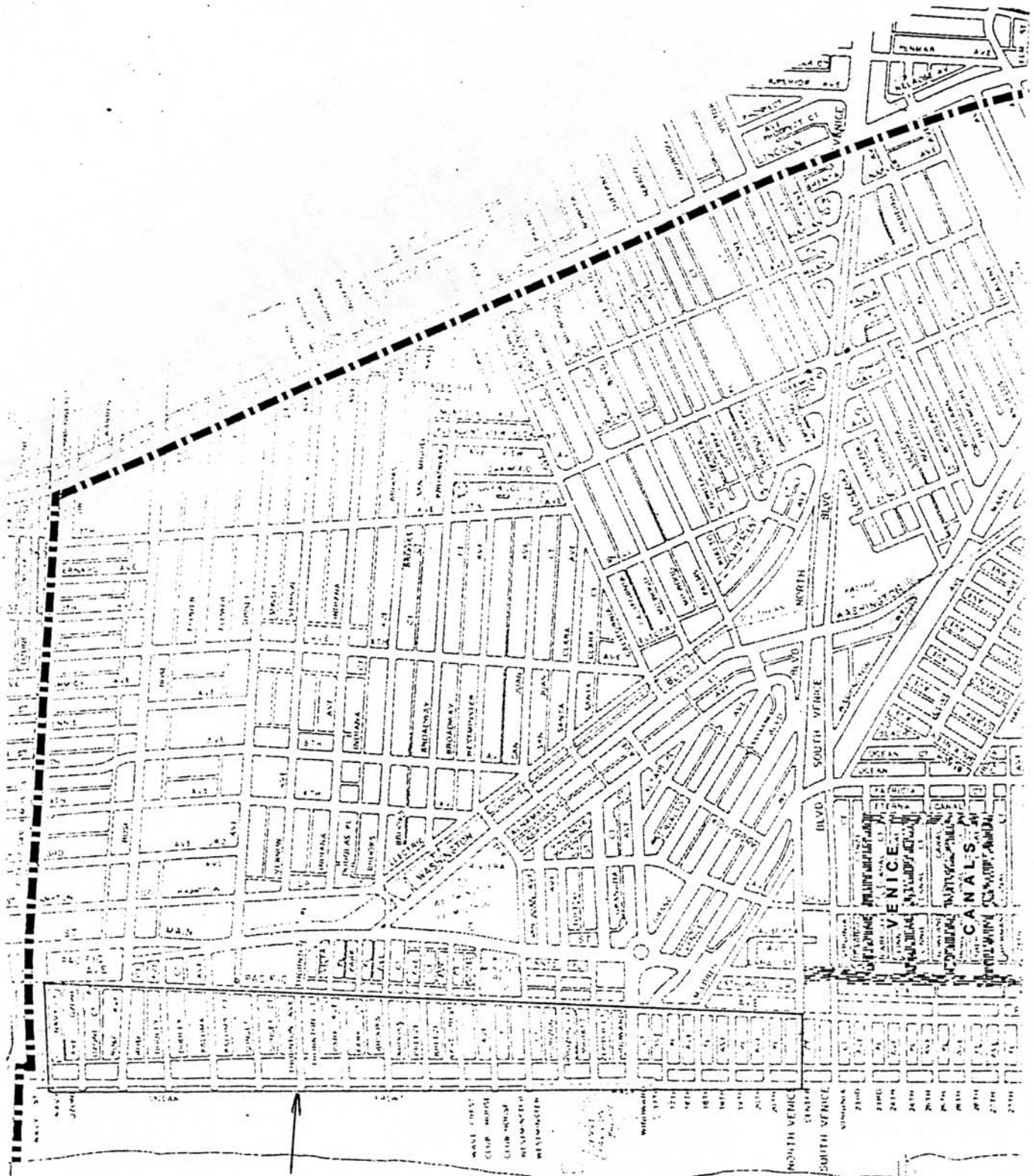
PRESENTATION FOR THE NORTH VENICE AREA B & C L.I.P.
GUIDELINES MEETING August 29, 1988

By

Tom Marshall
23 Horizon Ave.
Venice, CA 90291
(213)399-1975

Subject

COMMERCIALIZATION OF OCEAN FRONT WALK



VENICE NORTH BEACH
 (Area discussed in this presentation)

An estimated 6000-8000 people live
 in this quarter of a square mile.

P A C I F I C

0 400 800 1200 1600 Feet

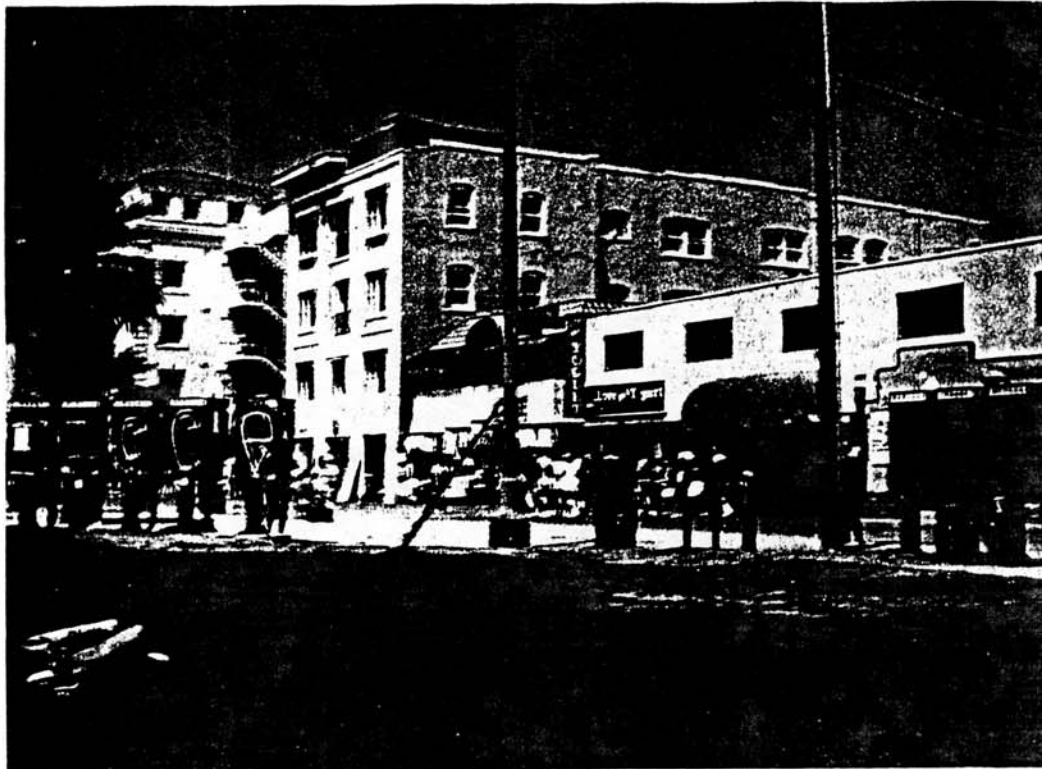
COMMERCIALIZATION OF OCEAN FRONT WALK

Commercialization along the Ocean Front Walk in Venice from Venice Boulevard to the Santa Monica line affects the residential community running north and south between these lines and bounded by OFW and Pacific Avenue on the west and east. This is a densely populated area (3236 residential mail delivery stops according to the Venice Post Office). The area has a strong sense of neighborhood. Emphasis heretofore has been on visitor accommodations, often at the expense of the people who live here. The estimated 6000-8000 people in the quarter of a square mile covered in this presentation alone are potentially a valuable market for neighborhood-oriented enterprises. The beach, like any other place of natural beauty, should be available to all who treat it with respect; but as residents of the adjoining community, which is itself part of the visitor attraction, we should assume the responsibility of proposing rules and regulations which will make our community comfortable for ourselves and help us to be gracious hosts to our visitors.

The following is a list of suggestions I feel will up-grade this part of Venice, known to some as North Beach.

1. Reduce anti-social behavior connected with public drinking by stopping the sale of take-out liquor within 1000' of OFW between Venice Boulevard and the Santa Monica line. An alternative would be to encompass only the area from Windward to Rose, thereby eliminating three of the most conspicuous sources of this blight.
2. Return to residential use all buildings and parts of buildings which were originally designed as residential.
3. In any future development, limit visitor-oriented businesses to the areas fronting on the parking lot at Rose Avenue and the stretch of OFW from Market Street to Venice Boulevard.
4. Allow no more than two pay-phones at the intersection of OFW and any residential street and limit the service so as to discourage their use for illegal activities.
5. Ban take-out-food service on OFW except withing the areas designated for visitor oriented businesses as delineated in suggestion 3.
6. Ban garage-type doors on all buildings facing OFW.
7. Ban loud radios on OFW and the entire beach area.
8. Raze the Pavilion but preserve the police substation and the meeting room.
9. Restore Windward to its original look. Encourage live theater, a small cinema, one or two moderately priced coffee shops with intimate cocktail lounges and other businesses consistent with the interests of an artistic beach community. The Pavilion area may serve as a small parking lot for theater and restaurant patrons. Businesses such as a beauty parlor, a barber shop, a stationery store would be welcome. Some of the existing businesses in the area which should be encouraged to stay are the bank, the post office, the pharmacy, the Windward Farms Market, the health food store, the hardware store, the laundry, the dry cleaning shop.

Some of the suggestions made here, together with related photographs, are expanded upon in the following pages.



OFW at Horizon

This is the OFW at the foot of Horizon Avenue. On one corner is the Sidewalk Cafe (not visible in the photograph) and on the other is the Westside Galleries. Horizon Avenue is a residential street and the managements of the cafe and the gallery are in sympathy with the residents.

The presence of ten pay-phones which are heavily used by drug dealers and gang members who congregate at this intersection are a detriment to the residents and aforementioned businesses. The Trading Post Liquor Store, at the corner of Horizon and Main, is within easy walking distance, and the quality of life on Horizon is severely compromised by disorderly people who use this street as a corridor in their restless treks between the Trading Post and the beach.

The photo shows some of the battered, graffiti smeared telephones. The grassy area and the nearby benches are amenities disproportionately used by derelicts and anti-social individuals. The police are well aware of these problems and seem unable to make lasting improvements.

This area would be greatly improved for use by the general public if (1) liquor sales at the Trading Post were stopped or strictly controlled, (2) all the telephones were removed or at least reduced in number to one or two, (3) the benches were removed at least until the area loses its appeal to the persons presently using it as a hangout, (4) the grass were kept wet during the hours when the area is most frequented by gangs and drug dealers.

Note: The telephones in the photograph have been removed and replaced by ten new phones since this picture was taken on August 18, 1988.

Introduction

This neighborhood study was undertaken to respond to the Planning Department's request for input from the community in formulating a Coastal Land Use Plan for Venice. The information contained in these documents focuses on the North Venice B & C Areas and is the result of weekly meetings for the period of approximately three months. The study group was composed of residents, property owners and interested parties listed on the next page. Everyone from the community was encouraged to join, whether through mailings, personal solicitation, or newspaper items.

In complying with the City's request, the group succeeded in addressing the issues of greatest concern. The written materials being submitted to the City Planning Department represent the positions and recommendations of subgroups and individuals from the attached list of participants. These documents represent the views of a number of dedicated, concerned community members who have given freely of their time in trying to express their vision of how growth and change can be directed and accommodated in Venice.

These documents offer the first step in the planning process. They are not the end result. The members of this group feel very strongly that the City staff should continue to keep us involved as the final Land Use Plan is developed. The force and numbers with which members of the Venice community turn out to Planning Department meetings is a good indication of the level of concern our community has for its future.

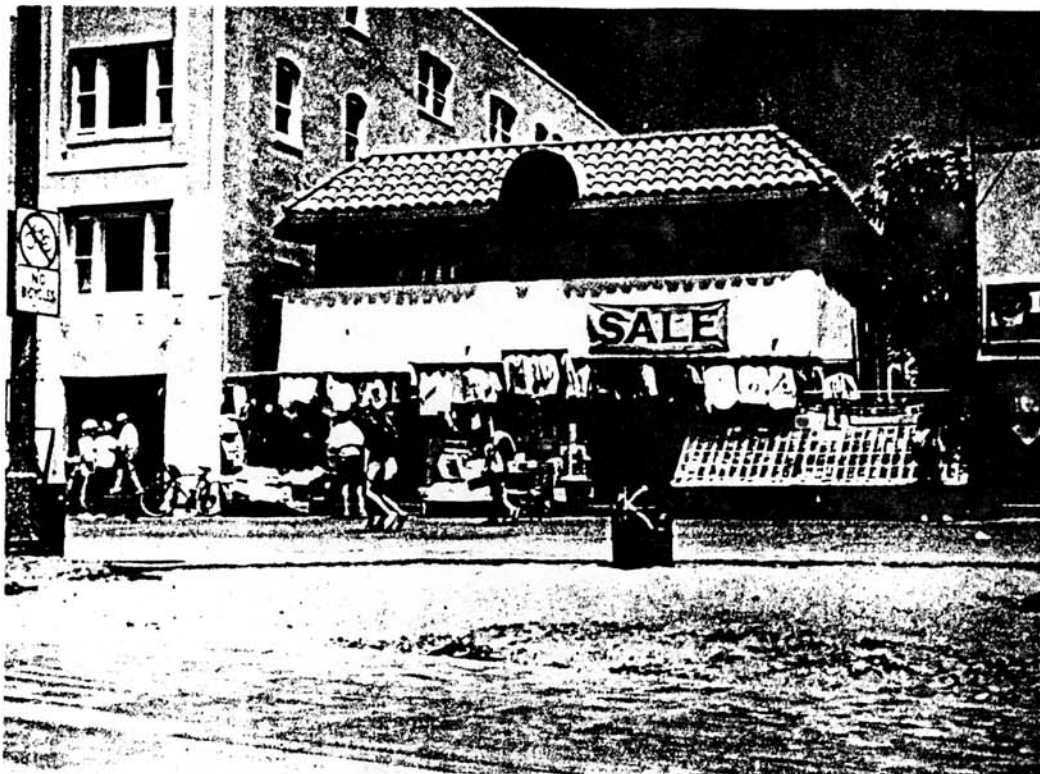
The input to this neighborhood study is divided into nine main sections

1. Residential:
 - Walk Streets*
 - Drive Streets
2. Commercial
3. Ocean Front Walk*
4. Windward Avenue*
5. Historical Preservation
6. Combined Areawide Development Standards
7. Parking and Traffic
8. Comments
9. Visuals (Presented on Display Table; to be submitted to City Planning Department.)

*The majority of time in our meetings was devoted to discussions of these topics.

10-30-88

/lk



OFW between Horizon and Westminster

This red-tiled building was designed as a residence. The lower floor is now shops for the sale of sunglasses, T-shirts and other items which are supposedly of interests to tourists. It was an attractive building before the change took place. The clutter of clothing hung across the front is an insult to one of the most beautiful beach-front areas in the world. It is likely that the original owner had difficulty renting to responsible people who would expect a clean and orderly area in front of their place of residence. A common response by police to harassed residents has been, "That's a bad area. You should move." Many have, in fact, moved. Apparently, indifferent or highly transient residents have taken their places and the soil grows more fertile for gang members and other social drop-outs.

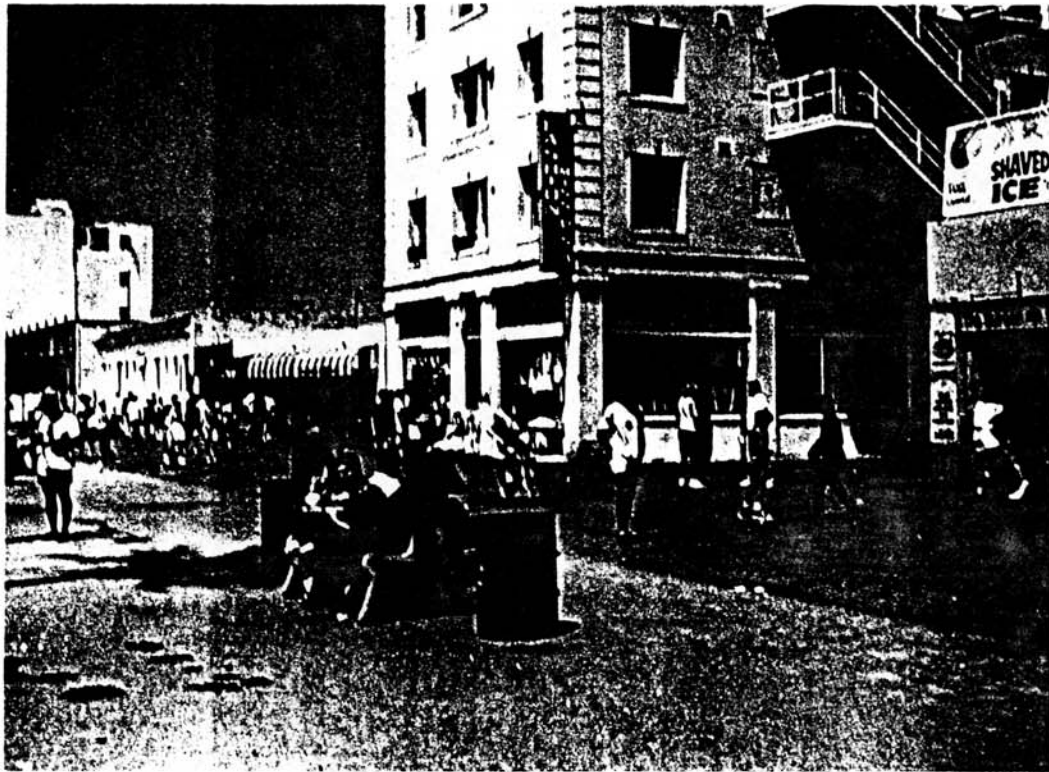
The corrective measures suggested under Exhibit 1 should be given serious consideration. In addition legal decisions which have tied the hands of the police in enforcing vagrancy laws should be re-examined. High density, pedestrian oriented communities are ecologically superior to the sprawling suburbs, but the notion that anything goes in such a community is 180 degrees off. The closer together people live the greater the need for mutual respect and good manners.



OFW between Horizon and Westminster

The garage-sized stores in the lower floor of the building in the foreground replaced elderly residents who had lived here for many years. The one-story building on the corner, The Market Place, was once a neighborhood grocery. It is now a conglomeration of small shops. Most of the shops are fast-food establishments which hand out food and drinks in paper plates, paper cups, paper bags and with handfulls of paper napkins. There are no places to sit down and eat inside. The patrons use the public benches in front, or stand around, or walk while eating. Much of the rubbish ends up blowing around the corners onto Horizon and Westminster Avenues. At night the Burger Tree, which hands out food directly to patrons standing in front of the premises, is a favorite gathering place for hoodlums. A few of the residents on Horizon hand-clean their own street two or three times each week. A considerable part of the trash comes from the fast-food shops, some of it is thrown from vehicles parked along the curb by disdainful visitors and some of it is dropped by thoughtless persons who walk back and forth between the Trading Post and the beach.

Fast-food service should not be permitted in areas where the ocean breeze can carry their trash into the residential areas. Parking on Horizon and Westminster avenues should be for residents only. Neighborhood grocery stores should be encouraged, but they should not be allowed to sell alcoholic beverages. Small grocery stores with competitive prices that do not sell alcoholic beverages are the corner-stone of the pedestrian oriented community. They should enjoy significant tax breaks and subsidies if need be to keep them in business.



OFW corner of Westminster

The lower floor of the tall building in this picture was once the location of the Lafayette Cafe. This comfortable little restaurant with its moderate prices, counter service and booths was the most popular meeting place for local people on the OFW. The proprietors and waitresses were all known by name to the patrons, and the patrons were treated like old friends. Its closing in 1985 brought much sorrow to the long list of regulars, many of whom had forged lasting friendships with people they had met there. Its regular clientele was from the neighborhood, but it was the type of place a visitor hopes to find in his travels. It is now a cluttered clothing store. Although the Lafayette was always busy it could not afford the triple increase in rent when its lease ran out. For those who take time to make observations, it is a mystery how the present establishment can afford the rent hike that drove out the Lafayette. Incentives should be put in place to encourage useful businesses which entail hard work and are short on monetary rewards.

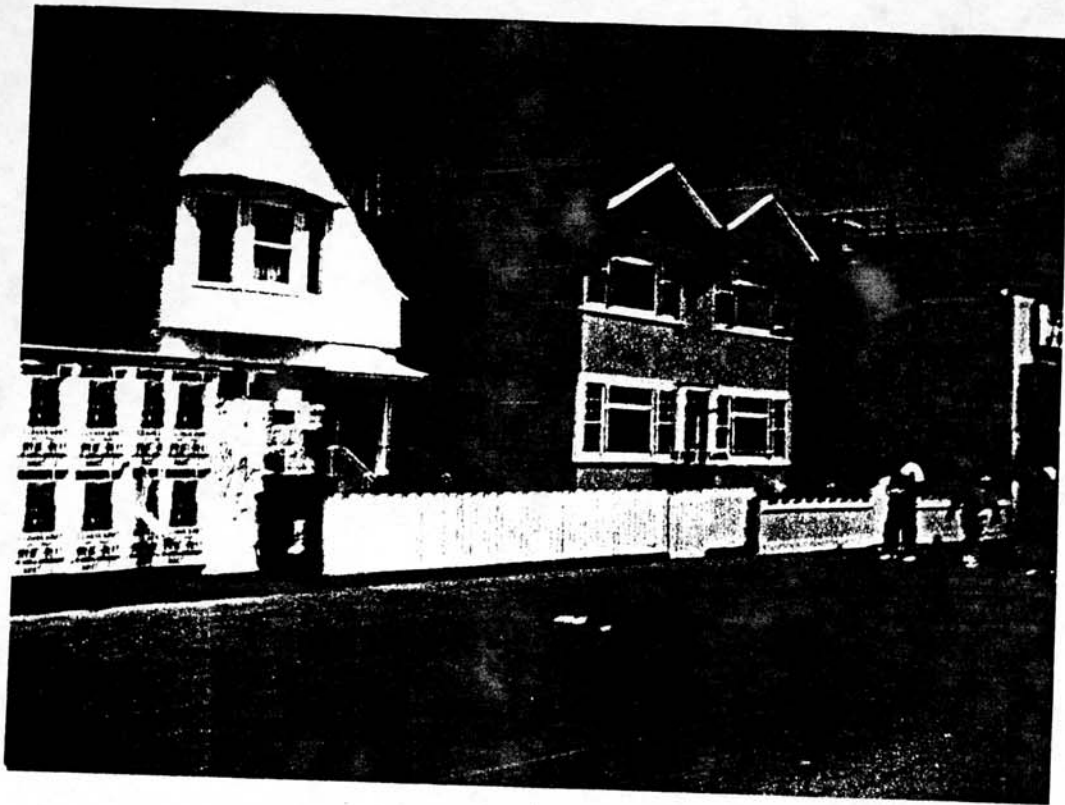
In the foreground of this picture a young man is seen sitting on a bench drinking from a wine bottle concealed in a paper bag. There is a strong likelihood that the wine (or whatever alcoholic beverage it is) came from the Trading Post or one of the two other nearby stores that sell take-out liquor. If a police car appears in the distance he will simply place the bag in the trash can beside him and retrieve it after the police pass. Efforts to up-grade the OFW will almost certainly be futile if the sale of take-out liquor is not stopped.



OFW corner of Wavecrest

This building once housed a friendly neighborhood bar. It was a very good gathering place for visitors on summer weekends. Like the Lafayette Cafe, it was a place where visitors could rub shoulders with the locals and enjoy the experience of meeting people who lived in a truly interesting community. Posters and T-shirts are sold there now.

A few well-run bars and cocktail lounges should be encouraged. They can be enjoyable places for neighbors and visitors to meet, and strong neighborhood organizations can influence the way they are run. Loud music should not be permitted, and people should not be allowed to hang around outside.



OFW between Brooks and Park

Here we have two of the few remaining residences along the OFW. The structure on the left, partially hiding the handsome old white house with a steep roof, is part of the new Park Plaza development.

The picture clearly illustrates the pushing out of residents whose presence served to maintain order and cleanliness along the Walk in days gone by. The charming little pagodas which were part of the Venice scene for generations have been vandalized and some have been completely destroyed as the number of permanent residents on the OFW diminishes.

If nothing else is done, these two old houses should be preserved as mementos of a more refined era which hopefully might return.

The joy of plunging into the waves breaking near the shore or listening to the sound of the surf or running along the water's edge or sunning on the wide, sandy beach or simply enjoying the wonderful view are the unique pleasures that Venice has to offer the city dweller weary of traffic and noise. T-shirts can be purchased anywhere and not even the most ardent carnival lover could tolerate a perpetual carnival at his door-step month after month and year after year.

Additional Material

WRITTEN COMMENTS

REGARDING THE DRAFT VENICE COASTAL LAND USE PLAN

NAME (Please Print) Wm. T. Marshall
ADDRESS 23 Horizon Ave.
Venice, CA 90291 (213) 399-1975

Please return by June 18, 1988

COMMENT

In the second paragraph of page 33 reference is made to restoring the Venice Pavilion. I have lived in the immediate vicinity of the Pavilion since the time it was built, and I can attest to the fact that it was an ill-conceived complex from the start. The theater part had poor acoustics, and it was cold and uncomfortable. Roofing it over did not help. The picnic area was never a popular eating place. Rock-band concerts held there were discontinued because of riotous behavior by the crowds and complaints of loud noise by area residents. When people speak of restoring it they simply do not know what they are talking about. There is nothing to restore! It is a monument to bad planning and poor taste squatting in the very heart of Venice. The police sub-station and adjoining meeting room may be worth keeping. At least, they lie below grade level and would not so blatantly obstruct the ocean view from Windward Avenue.

Windward should be restored to its early 1900's look. It should have a cinema and a little theater for stage presentations. A moderately priced restaurant with counter service and an intimate cocktail lounge would be welcome. If necessary, a parking lot for theater and restaurant patrons might replace the Pavilion. It could not be a greater eyesore.

The area to the east of the Venice Circle has the potential of becoming a fine urban setting with handsome 3 & 4 story buildings grouped around a triangular park which would replace the Post Office Annex. This would call for urban renewal beyond the scope of the present plan, but I believe the possibility of such development should at least be recognized at this time.

I am skeptical of what may result from assigning a mile-long stretch of Ocean Front Walk to Artcrafts. Is it reasonable to expect persons sustaining themselves by making baskets or candles or art needle work to pay the rent or mortgage on choice Ocean Front property? Mindless talk about arts and crafts was the forerunner to conversions of dwelling units to commercial uses. Clothing stores, trinket shops and fast-food establishments have displaced valued neighborhood businesses which helped give the community character and flavor in the first place.

SIGNATURE

Wm. J. Marshall (Tom)

Copy to:
Councilwoman Ruth Galanter

Venice, California.
May 18, 1988

Regarding:

B.Z.A. Case No. 3774
CP-111
ZA 87-1359(ZV)
CDP 87-044

Board of Zoning Appeals
Room 504 City Hall
Los Angeles, CA 90012

1415 Ocean Front Walk
Venice Planning Area
Council District No. 6

Dear Sirs,

Please deny a zone variance in the above-noted case. Traffic, trash and parking problems are already very much out-of-hand because of the inappropriate commercialization which has taken place here at North Beach in Venice. The property in question backs on Speedway, an alley clogged with vehicles and overflowing dumpsters. Access by emergency vehicles is a serious concern.

This community has suffered enough abuse. Delivery vehicles and shop owners blithely drive onto Ocean Front Walk. Trash from fast-food establishments blows into residential areas. Businesses cherished by the community, like the old Lafayette Cafe, become cluttered clothing stores and trinket shops. By night these businesses become a row of graffiti-smearred garage doors and skeletons of the make-shift vending structures which have risen on vacant lots. They form an apt background for the comings and goings of gang members and drug dealers, who are very much at home amidst the filth and ugliness. Sad to say, there are fewer and fewer local people to protest as the onslaught of money-hungry, non-resident, entrepreneurs convert former living units to more and more junk stores. They will surely fail in time, but what a legacy they will have left!

Contrary to statements made by Mr. Dror's lawyer at the hearing before the Zoning Administrator, not all the occupants of the upper floor at 1415 OFW are elderly people without cars. There are eight parking spaces behind the property and most of them are regularly occupied.

I have nothing against Mr. Dror personally. I am simply taking a stand against a process which I think should be stopped if not reversed. The realities of our time call for more pedestrian-oriented places throughout the city to reduce the environmental disaster being brought on by over use of the private automobile. Furthermore, it is doubtful that the majority of people who converge in Venice are seeking beach-oriented activities at all.

Sincerely yours,

Copy 101
Councilman Keith Galanter

Wm. T. Marshall
Wm. T. Marshall
25 Horizon Ave.
Venice, CA 90291
(213) 399-1975

Venice, California.
July 18, 1985

Office of Zoning Administration
Room 600 City Hall
Los Angeles, Calif. 90012

RE: CASE NO. ZA 85-0515(ZV)
ZONE VARIANCE

Dear Sirs,

This is to register my protest as an affected and interested 20-year resident/owner in Venice to the application of Signet Key Systems, Inc/Ju Bok Lee to permit open air sale of merchandise in front of 1309 Ocean Front Walk in Venice.

A rack of sun glasses and a clothes line across the front of the aforementioned address have replaced what was once a neat four unit dwelling. Behind them are quarters jam packed with clothing. And now the applicant wants to extend his business onto the Ocean Front Walk which is a public walkway.

The application should be denied:

- (1) It will create an obstruction to pedestrian traffic which is particularly heavy at this location on weekends.
- (2) It will encourage other merchants to spill out upon the sidewalk.
- (3) It is not a beach related activity.
- (4) The aesthetic qualities of the beach are degraded.
- (5) The real needs and values of the host community are ignored.

Sincerely yours,

Wm. T. Marshall

Wm. T. Marshall
23 Horizon Ave.
Venice, CA 90291

(213) 399-1975

Copy to Councilwoman Pat Russell

1.0 Artcraft Zone (mixed commercial, light industrial, residential)

Commentary: Ocean Front walk cannot be treated as one piece with one zone as it has various uses in the 1 1/2 miles being considered- some of the areas are used mainly as residential and have low foot traffic whereas other areas are dense with visitors, have parking lots, and are highly unsuitable as residential areas. The Ocean Front Walk area has been used as a commercial area since the inception of Venice with the end of Venice Blvd. formerly being a pier, the block between North & South Venice having a dance hall, beer hall, and auto repair shop before it become an empty lot and then low cost housing building it is now. Some areas have become more commercialized but others have become less. The distribution has changed somewhat over the 80 year history of Venice but through it all the Ocean Front Walk area has remained primarily a commercial area.

Objective: There is a need to have an overall guideline for Ocean Front Walk without destroying the nature of the area: it is a haven and the only place in Los Angeles where artists of all kinds can express themselves to an open audience; a place where there is a open air "street fair feeling"; a place where the city gets to see itself on leisure time; it is the main recreational beach of Los Angeles- sporting Paddle Tennis courts, Weight Lifting, Basketball, Gymnastics, Handball Courts, childrens play ground, picnicking area, rollerskating, skateboarding, bike riding and beach going

Washington to Venice Blvd. with the exception of the 1st block north of Washington is a **residential area** with no commercial buildings or businesses

Venice to Club is **primarily commercial** with only 3 buildings being used as residential on their 1st floors

Club to Breeze the block is three quarters residential with one building having a market on the ground floor.

1.1 Density

Objective: To not intensify the use of Ocean Front Walk and to meet the needs of the beach, recreation areas, and Ocean Front Walk visitors

Commentary: FROM A LETTER BY RUTH GALLANTER, Councilwoman, Sixth District August 11, 1988

"What of the supposed over-commercialization of the beach area? There are some aspects of it I dislike as much as you do. But I think that some people do the question a great disservice by trying to over-simplify the answer. Venice is not simply a residential community with a wide beach;

it never was. It was created as a visitor/summer resort, and has retained much of that flavor through all of the transitions. Anyone who wants it to be the picturesque equivalent of a quiet Mar Vista neighborhood at the beach is denying its history. Likewise anyone who wants it to be the next Laguna Beach.

I would hate to see us fall into the trap of turning Venice into an enclave for the rich by virtue of our attempts to control what happens here. In the battle to keep it from becoming Laguna beach, I am convinced it is every bit as important for Venice to retain its appeal for a wide variety of beach visitors as it is for us to preserve and create affordable housing opportunities."

Guideline: C1 zoning ground floor commercial with residential on upper floors- Venice to Breeze. In areas where the commercial use is not dense the grandfathering in perpetuity of the existing commercial uses with the right to rebuild in any scenario of the commercial establishment. Where there are large housing buildings in the C1 zone grandfather them in to their proper zoning e.g. R4.

1.2 Height

Objective: To preserve the scale of the neighborhood.

Commentary: The existing buildings are mainly one, two, and three stories with the ground levels being used for commercial purposes. Parking regulations for commercial properties include such needs as loading docks, and high numbers of parking spaces depending on the type of business. Places of business typically need higher ceilings than residential for both functional, structural, and esthetic reasons. And typically need more room for storage and garbage. The existing height limit encourages the building of flat roofed boxes in an attempt to maximize building area. A combination of restrictions and incentives should be developed to prevent the boardwalk from becoming a homogeneous group of boxes with security shutters. Thirty five foot height is desirable for commercial structures to allow for high first stories.

Guidelines: Thirty five foot maximum height on Ocean Front Walk. Architectural projections including chimneys, roof access enclosures, and towers and penthouses of no larger than ten percent of the lot size may reach a height of forty feet.

1.3 Setbacks

Residential Properties (Washington to Venice)

- 1.3.1 These should be similar to walk streets with some provisions
- 1.3.2 for higher fences because of the much greater numbers of
- 1.3.3 people walking by these properties so that these owners may retain more privacy

Commercial Properties (Venice to Breeze)

1.3.4 Front Yard Setback

Objective: To produce a street edge compatible with the existing fabric. To discourage the congregation of passers by or places where people may loiter or set up vending areas.

Commentary: Ocean Front Walk is a very high foot traffic area; in the commercial zone people tend to fill all available areas. The providing of a setback from the front property line of a building, in this area, will effectively take that piece of property away from the lot and will serve no useful purpose. As this is on the ocean front there is wide open space to the west.

Guideline: No front yard setback commercial zone.

1.3.5 Rear Yard Setback

Objective: To allow delivery of commercial goods, to allow the clean storage of waste from businesses, to allow access to parking.

Commentary: Because of the complexities of operating the various businesses; and the requirements of such things as loading docks, higher parking requirements than residential zones, greater building code requirements owners of these lots must be given maximum use of their substantially undersized lots to provide all of the code requirements that were developed with 5000 square foot lots in mind.

Guideline: No rear yard setback.

1.3.6 Side Yard Set Back

Objective: To provide adequate light and air between buildings. To encourage architectural projections and a heightened level of visual interest.

Commentary: Current code allows commercially zoned properties to build to the property line. In an ocean front area this can lead to many buildings having walls that touch and creating a "row house" effect blocking out light and air and a feeling of spaciousness that should exist on the ocean front. Side yards on ground level in this area are problematic that they create spaces where people urinate and defecate,

spaces where people drink, do drugs, and sleep overnight. Side yards also become garbage collectors with trash blowing in from the Ocean Front Walk with the coastal winds. A compromise effect needs to exist on Ocean Front Walk.

Guideline: On the first level commercial projects may build to the property line. On upper levels they are to have setback that equal 10 per cent of the lot width on either side.

1.3.7 Architectural Projections

Objective: To encourage varied massing, architectural articulation, and use of scale giving elements.

Commentary: It is important to continue and enhance Los Angeles's main recreational beach front as a unique architectural attraction. Every effort should be made to encourage the use of arches, castings, ornaments, and other embellishment to continue the unique architecture that embellishes Venice.

Guideline: NEED LANGUAGE HERE

1.4 Streetfront

Objective: Create an ocean front walkway that is both attractive and functional in this very heavily used area.

Commentary: The westward portion of Ocean Front Walk should be properly landscaped and maintained as a joint venture between the merchants the city and county. In places where the Boardwalk is hit with waves and logs during storms an appropriate retaining wall should be built. Along the parking lots at both Washington and Venice appropriate separations, to the walk should be built, masonry planter boxes should be built around trees, appropriate ice plant type foliage should be planted in the planters, we should also provide benches in areas that do not have them. The sprinklers along the parking lot should be repaired and the grass should be watered and maintained. Property owners should be encouraged to have planters on their buildings with plants like bogenvillia and have vines growing on the buildings for more green. Some form of agreement should exist between the property owners and the government for maintenance and repair of the westward portion of the walkway.

Guideline: NEED LANGUAGE HERE

1.5 Trash

Objective: To have trash out of sight on speedway and to have it in such a way that it does not encourage dumping by visitors or residents.

Commentary: Trash bins are the collection method for all businesses on Ocean Front Walk; an overfilled trash container is the first reason for someone to rummage through it and spill garbage onto Speedway. A business must pay for each collection about ten dollars so overfilled containers is cost saving but not community oriented. An unlocked container always attracts garbage from adjacent residential buildings. Some of the trash pickup companies provide locking containers free to their customers. Trash containers belong on the property of the trash producer and do not belong on city property like so many are.

Guideline: Enforcement of keeping trash containers on private property. Requirement of not overfilling and must have locking containers.

1.6 Lot Consolidation

Objective: To maintain the smaller building scale of the residential area and to stop the consolidations of commercial areas into shopping malls.

Commentary: The Washington to Venice residential area has developed into an attractive single lot area where lots have been consolidated the buildings are unattractive to the ocean front. In the commercial zone Venice to Breeze multiple lot consolidations have produced monstrosities like the Bath House.

Guideline: No lot consolidations in residential area. In commercial zone maximum of two lot consolidation; a common wall construction shall be allowed to extend no more than fifty feet on street frontage or two lots whichever is less, with the exception of subterranean parking which may go to the lot lines. In the commercial zone, two adjacent properties may consolidate parking.

Group B/C
from Bob Goodfader

.PROPOSED FUTURE GUIDELINES FOR THE OCEAN FRONT WALK AREA

VENDING

Objective:

Vending at the beach is well established having been in existence here for several decades. It fits in well with the free-wheeling and colorful ambience of the area. Vendors provide a unique and valuable service to both residents and beachgoers alike and should be encouraged to continue.

Commentary:

Vending has thrived in Venice since before World War II. It's success and popularity is attested to by its long lifespan. It serves residents with low-cost and sometimes unique goods and beachgoers by providing a sufficient number of flexible outlets to accommodate their changing needs. The multi-million dollar annual income which, in part, flows back into the community, the taxes and jobs it generates are vital to the economy of Venice. Estimates of the number of people directly dependant on vending (vendors, families, employees, etc.) exceeds a thousand. While some products sold by vendors are commonplace others are unique and noteworthy and many are the products of local artists and artisans. A helter-skelter appearance is often noted on some of the vending lots but this can be corrected and should not detract from a consideration of vending's historic and economic significance to the community.

Guidelines:

Vending should be allowed to continue in Venice along Ocean Front Walk and adjacent areas as it is in keeping with Venice's history and lifestyle. When and if, buildings replace the open air vending lots these buildings should be of sufficient size and scope and type to accommodate a maximum number of vendors displaced from those lots to minimize the hardship that would ensue. During the interim efforts should be made to encourage a somewhat uniform appearance of the vending stalls on each lot but allow lots to vary to encourage artistic expression and reflect local color. During the period when a lot remains vacant, vending should be encouraged.

PARKING

Objective:

The amount of parking should be increased in the Venice area. There is a serious deficiency of parking at present.

Commentary:

The great shortage of parking spaces and facilities in Venice is inconvenient and aggravating to residents, beachgoers, vendors and merchants alike. We believe that increasing the number of available parking spaces will not necessarily increase congestion at the beach but will certainly ease the 'crisis' for all those presently requiring parking. We also suggest that an improved public transportation plan to accomodate non-resident beachgoers would be of great benefit to local residents and commercial interests as well (SEE TRAFFIC AND TRANSPORTATION).

Guideline:

Allow new buildings in Venice to be of sufficient size and scope to allow significant increases in number of parking spaces. Increase construction of non-beach area parking lots and provide adequate public low-cost transportation from those lots to the beach. Encourage non-local people to use these "shuttle" services on weekends, holidays and during the summer.

TRAFFIC AND TRANSPORTATION

Objective:

Traffic should be reduced, particularly on weekends and during the summer by all means possible.

Commentary:

Although traffic control is a highly technical field, certain common sense answers come immediately to mind. Increased and convenient public transportation during peak hours would seem to be a reasonable part of any solution. Reducing the size of existing parking spots to increase their number and reflect the greater amount of small cars on the roads today. A definitive transportation study by experts should be undertaken to find other effective ways to reduce the number of vehicles entering the area without restricting any citizens rightful access to the beach areas.

Guideline:

Commonsense approaches should be implemented NOW; downsize as many parking spaces as possible, provide more off-beach area parking and provide more public transport. These steps

should be implemented immediately as they cannot possibly harm any future master plan and yet can provide benefits now.

TRASH & TRASH RECEPTACLES

Objective:

Trash should be kept out of sight and smell insofar as possible but depositories should be numerous and convenient enough to encourage public use.

Commentary:

Trash at the beach is one of the more obvious and annoying problems to all. It is not caused by any one group but by many. It would appear that the majority of trash is deposited by beachgoers rather than locals.

Guideline:

More receptacles, more conveniently placed and a more frequent collection system is needed, particularly on weekends, holidays and during the summer. A system that is, in part, voluntary is also needed. Local merchants, restaurants and vendors might be asked to provide more receptacles on their areas and provide for the transportation of the contents to major collection points at frequent intervals. An experimental plan in this direction is now being tried out by the local vendor community. Increased use of technology (i.e. compactors) and recycling approaches would also be useful.

CRIME REDUCTION AND LAW ENFORCEMENT

Objective:

The extent and scope of crime at the beach is a perceived serious problem but the true extent and scope is not widely known. Local law enforcement should provide a series of briefings to publicize the true facts and request community input and assistance in making the area safer. We should take a hard look and demand MORE police services to combat the problem.

Commentary:

The beach is thought to have been a high crime area for some years but the average citizen is not actually aware of the real figures and the comparison to other areas such as the Marina because of lack of communication between the police and the community.

Guideline:

An independent citizens committee using law enforcement personnel as advisors not as moderators or facilitators should look into the situation and make recommendations directly to the City Council and the Mayors office. Questions to be asked, among others are: (1) how many officers are assigned to Venice, particularly during off-peak hours. (2) Do we have the same officer/citizen ratio as other areas such as Santa Monica and the Marina. (3) Is there adequate protection on the beach area, particularly after dark? Many citizens perceive sufficient police presence during peak periods but have commented on the lack of such presence on the boardwalk area at night. (4) Who should be patrolling the actual beach. Should park rangers be assigned in addition to police officers? Should private security be added to augment government resources?

IV

WINDWARD AVENUE

From L. Wertheimer,
Group B/C

VENICE LAND USE PLAN COMMUNITY WORKSHOP

WINDWARD AVENUE ARCADE

GOAL:

To maintain, preserve and restore the existing arcade buildings in the Windward Avenue area and to require new developments to replicate the arcade. The original scale of Windward Avenue should also be preserved and reintroduced in new developments.

COMMENTARY:

The vision that Venice's original developer, Abbot Kinney, imported to this Southern California seaside resort in 1905, was rather ambitious. In the early years, Venice was full of tourists, amusements, and recreational activity supported by a richly interwoven urban architecture that supported them. Today, although Venice and particularly its Ocean Front Walk are still a vibrant scene of visitors and activity, there remain only scattered fragments of the original architecture. However, even the few remaining buildings, some in much disrepair, still seem to be richer and more successful both aesthetically and urbanistically than any of the newer projects.

Windward Avenue was and is the center of Venice. It deserves a more noble appearance, one that we, as residents of Venice, would be proud to share with the countless tourists and beach goers that visit from all over the world.

Architectural diversity is an important aspect of Venice. This should continue to be encouraged but at the same time the order and strength of the arcade as a unifying element within the center of the diverse fabric of Venice seems both appropriate and pleasing. The arcade also provides climactic protection to the pedestrian. Although rare in the United States, these covered sidewalks are found in many parts of the world, and in particular, Venice, Italy, the source of architectural inspiration for Windward Avenue. We are fortunate in our community to still have enough left of the arcade to initiate planning measures that would require a builder to reconstruct the arcade in the proposed locations shown below and to protect the already existing arcaded buildings which are historically an integral part of Venice.

The scale of Windward Avenue is another important aspect of this area of Venice. Windward Avenue, measuring 85' across, is one of Venice's widest streets. It was originally designed to accommodate buildings ranging in height from approximately 49' to 62'. These buildings had many stylistic variations and roof treatments such as ornamental parapits, gabled roofs, spires, sloped tile roofs, etc., but they all had the consistent arcade and a level ornamental decorative band just above the arcade.

IMPLEMENTATION

Height and Density: It should be made clear that although this proposal allows for more height than in other parts of Venice, it in no way encourages higher density. The allowable F.A.R. of 1.5 should either be maintained or reduced to 1.2. It should also be made clear that the height allowance is only for the portion of Windward Avenue between Pacific and Ocean Front Walk and along Ocean Front Walk up to and including the lots fronting the two alleys, Zephyr Court and Windward Court. There should then be an intermediate height allowed between the lower scale lots for the rest of Venice and the higher Windward avenue area. (See the attached map "Article A" designating these areas.) The permitted heights in proportion to the allowable F.A.R. should encourage Architectural variation.

Arcades: New buildings within the area shown on the attached map, Article A, shall be required to build the arcade in alignment with the original arcaded buildings. In all cases, the upper stories of the original buildings extended out over the sidewalk and arcade. This precedent should be continued.

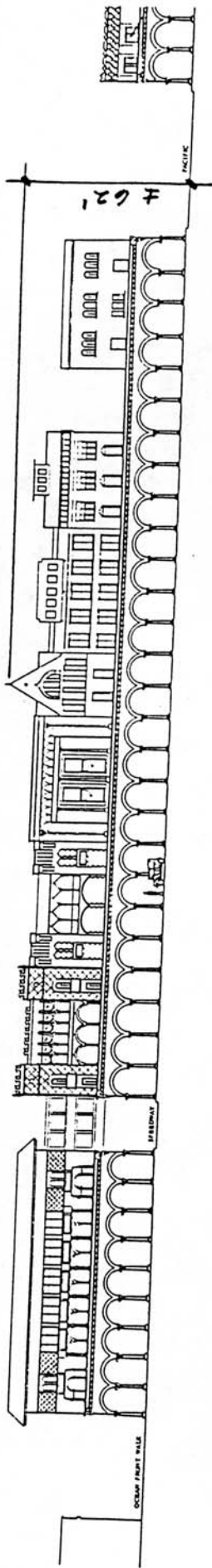
There is a slight slope to the East-West streets, Market Street and Windward Avenue (the high point at Pacific and the low point at Ocean Front Walk). The height of the columns and continuous level ornamental band above the arcade arches vary from ground level. This level variation measures approximately 8'-9" from top of curb to top of column capital, at the North-West corner of Windward Avenue and Pacific and 12'-0" at the North-East corner of Windward Avenue and Ocean Front Walk.

Preservation of Windward Arcades: No demolition of any of the existing arcaded buildings in Venice shall be allowed. Remodeling of interiors or additions above or behind the arcades are allowed as long as they are consistent with all other guidelines for new construction or major additions or remodels. The arcades themselves must be kept as are or restored to their original appearance.

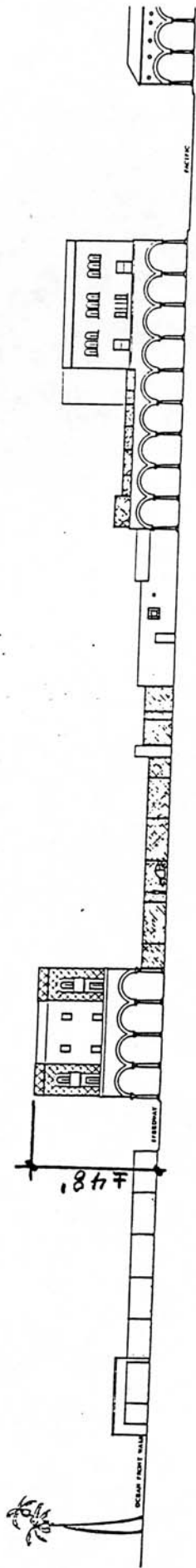
The new arcade should be consistent in detail and proportion to the original ones. This should include column, capital, vault, and arch details. Since there is little left of the original decorative band above the arches except the line of its location, the location should remain the same in new buildings. However its design along with the rest of the building above and behind the arcade is at the discretion of the architect and builder but must remain within the city and community guidelines.

Submitted By:

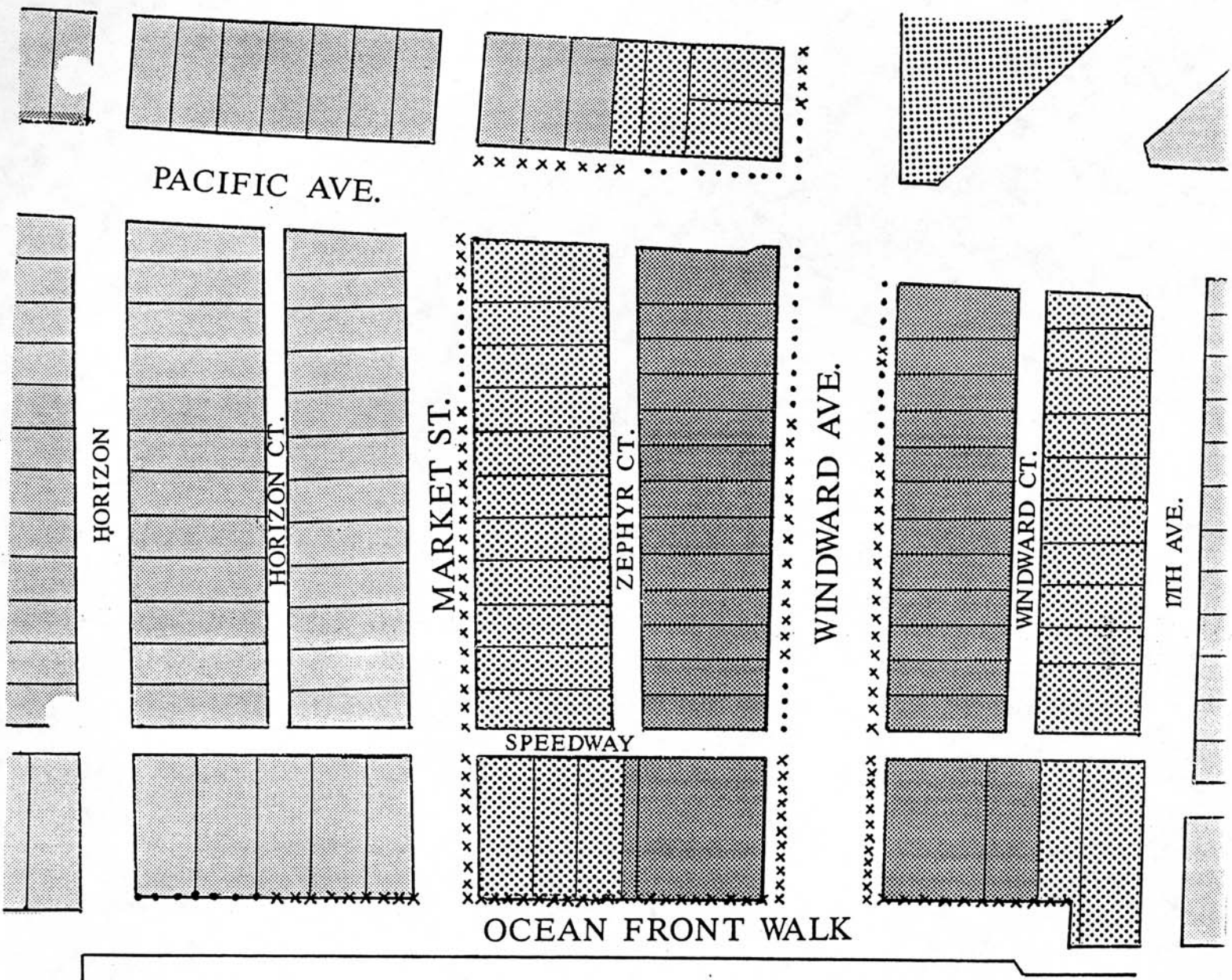
Lewin Wertheimer
26 October 1988








WINDWARD AVENUE - 1925



WINDWARD AVENUE - PRESENT



LEGEND

-  EXISTING ARCADE
-  NEW ARCADE
-  LOW ZONE - ± 30 feet (per group decision)
-  MEDIUM ZONE - 30 feet - 45 feet
-  HIGH ZONE - 45 feet - 52 feet plus (10 feet maximum for sloped roofs, towers of no more than 100 s.f. in plan area, parapits and gables)

PROPOSED FUTURE GUIDELINES FOR THE WINDWARD AVENUE AREA

PARKING

Objective:

The amount of parking should be increased in this area. There is a tremendous deficiency of parking at the present.

Commentary:

Parking is lacking throughout Venice, and the Windward area is no exception. Both commercial and residential parking needs should be accommodated; commercial at commercial rates, and residential at residential rates. In addition, we do not believe the idea that more parking brings more cars. The cars are there, and any additional parking is beneficial. There should be some incentive for builders to give as much extra parking as possible.

Guideline:

The increased heights arising in conjunction with the restoration of Windward will decrease respective FAR's. The additional buildable space should be devoted to public servicing uses such as parking or low income housing. The use of any unused city land, city air space, or underground city land should be utilized. The use of valet and security parking is also a positive. These additional uses will both add parking spaces and make the spaces that much more attractive for the local residents to use. The other positive part is that the additional visitor parking can be used during the off-peak hours and months for the local residents to use. An important point is that most of the time there should be an excess of parking in any new building if this guideline is used.

HEIGHT

Objective:

The sky line of the 1920's and 30's should be recreated as much as possible. The basic concept of the original Venice plans should be kept and expanded if it is at all feasible.

Commentary:

A consistent sky line along Windward Ave. would not only recreate what once was, but it would also be more pleasing to the eye. In addition, it should be a community's dream to have architects design around this historical concept. The finished product would be both economically feasible and visibly beautiful, greatly differing from the one story cement bunkers that can easily be built today.

Guideline:

There are city guidelines that should be adhered to and at the same time the present buildings should be designed into the scheme, so that the sky line will look as beautiful as possible and as consistent as can be attained. At the same time, the FAR's should be used and possibly lessened as the height increases. Perhaps increased parking and low income housing should be used to make the lesser FAR work.

TRASH & TRASH RECEPTACLES

Objective:

Trash should be kept out of sight, sound, and smell as much as possible. Present day technology in this area should be utilized.

Commentary:

There is no reason for trash to be in public view. It is unsightly and detracts from the beauty of an area. In addition, adequate provisions for trash are not expensive.

Guideline:

Areas out of public view must be provided for all trash receptacles. To keep the amount of trash to a minimum, bonuses may be provided for those using technology in this area. Trash compactors are an example of such a technology. Recycling could also be a means to obtain bonuses.

HISTORICAL PRECEDENCE

Objective:

The creation of a "Windward Avenue Historical District" should be considered. This district would be in line with the original Venice plans and would tie together new construction with preexisting historical buildings to reconstruct the atmosphere of the 1920's and 30's along Windward Ave.

Commentary:

Presently, Windward Avenue is a fairly unsightly mixture of old and new, tall and small, arches lining lots and empty lots, etc. The creation of a historical district along Windward would attempt to add consistency to the area with an underlying theme. The arches could be rebuilt, all to the same height, creating a line of reference to onlookers. A consistent sky line could be reinstated, thus adding perspective to the area at a proper scale. Buildings, of course, could be built with a historical flavor to mesh with those already standing, such as the St. Charles Hotel.

Guideline:

Incentives could be used to promote the creation of this historical district. An alternative is penalties. For instance, a smaller allowable FAR, or large setbacks, etc.

LINE OF SIGHT

Objective:

The restoration of the sky line along Windward Ave. will add beauty rather than remove the view of passers by.

Commentary:

Windward is a wide street (wider than Main Street) measuring 86 feet across. From the opposite side of the street, an angle of sight of about 28° is measured. This is much less than that existing on many walk streets as well as some larger projects.

Guideline:

The increase in height on some lots on Windward to match the now existing height of buildings should not prohibit the reconstruction of Windward as a historical area with buildings of their original flavor.

FAR & DENSITY

Objective:

Interesting and beautiful buildings can be promoted by controlling such factors as FAR and density.

Commentary:

Low FAR's may be achieved because of the proposed increased height allowance along Windward. This will in turn promote less dense buildings, as well as buildings with greater architectural variety. "Big Boxes" are thus discouraged.

Guideline:

The maximum FAR in the Windward area should be 1.5.

TRAFFIC

Objective:

As much as possible, traffic should be stopped before it gets to this area. That traffic which does come to the area should be provided for by creating a traffic flow that is as smooth as possible.

Commentary:

Traffic flow is a very involved subject, requiring great amounts of research as well as expertise. A traffic study for the Windward Ave. area could address the above topics of traffic control and possibly provide some solutions to problems that exist. Such solutions might include having all incoming traffic enter via surrounding alleys and leave via Windward Avenue. Another solution is satellite parking with shuttle busses servicing the Windward Area. Satellite parking and shuttle busses are a viable solution to traffic and parking problems in other areas of Venice as well.

Guideline:

A traffic study should be performed by professionals concerning Windward Ave. and the surrounding areas. Its findings should be heeded on a timely basis. In addition, the existing Venice shuttle should continue its operations.

LOT CONSOLIDATION

Objective:

The promotion of Windward as a historical area will require lots of similar size to those that existed when the first buildings were there.

Commentary:

Lot consolidations can often be a problem. However, for the proposed historical area, they may be future solutions toward reconstruction. Our investigations have not found any possible future problems in this area, however, the city may wish to look into this.

Guideline:

Allow lot consolidations that allow the promotion and enhancement of the historical nature of the Windward area.

SUBTERRANEAN PARKING

Objective:

New parking should be promoted wherever possible.

Commentary:

Underground parking, while expensive to build, allows for other uses for above ground space. To recover some of the costs of building below ground, builders could be allowed to build under city owned land, such as under the public right of way, or even under the Venice Pavillion.

Guideline:

Allow builders to construct parking in city owned property to make such a project more cost effective and thus promote more parking in the area.

LANDSCAPING & MAINTENANCE

Objective:

The proposed historical district should be enhanced with landscaping (such as palm trees) and the beatification should be maintained.

Commentary:

Landscaping is a welcome part of any endeavor to create a nicer area. It is not too costly and its benefits are great. Along with landscaping must come its maintenance; many beautification projects have been all but gone a short time after they were finished.

Guideline:

There should be incentives for landscaping and its maintenance around new projects.

VENDING

Objective:

Vending at the beach goes along with Venice's carnival-like past. This vending provides a unique service to beachgoers which is characteristic of the nature of Venice and should be allowed to continue.

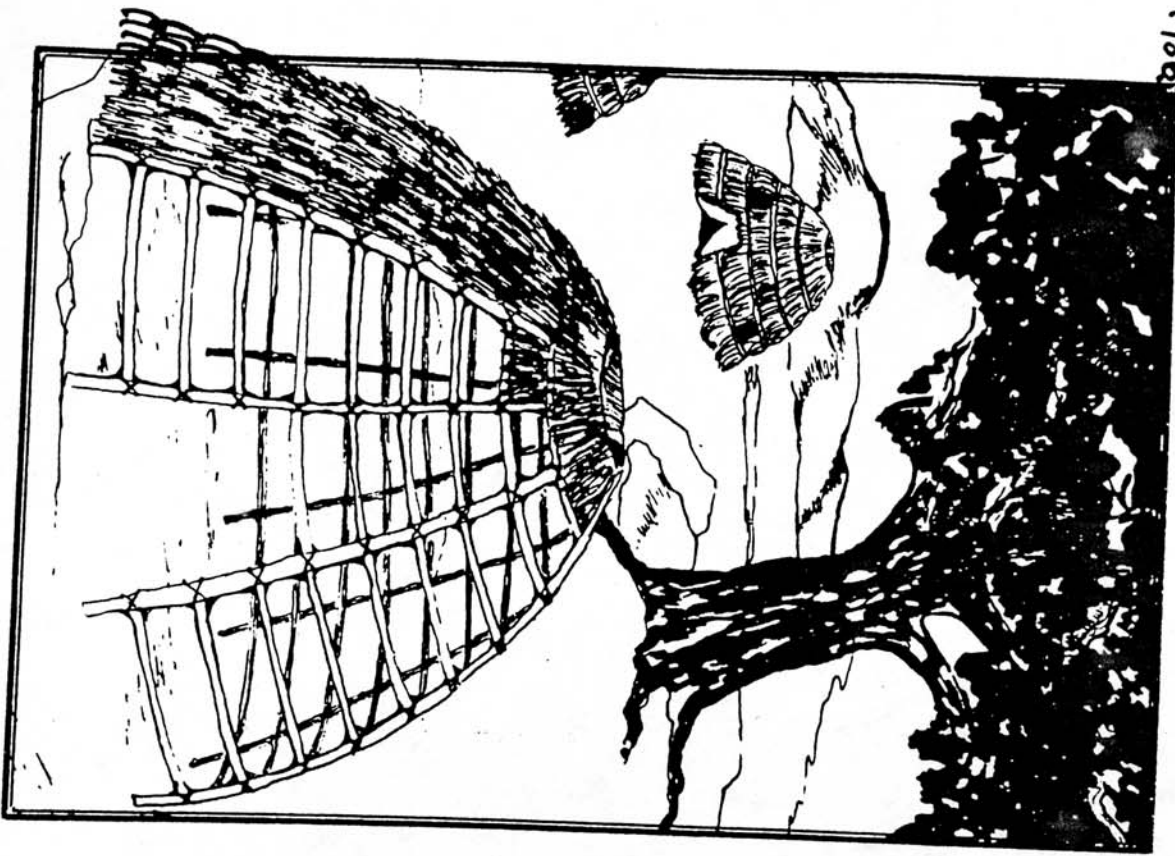
Commentary:

Vending has existed in Venice for several decades. Its success is accounted for by the fact that it is still thriving after all of these years. It provides a service not only to beachgoers, but also to the many who are employed in this "industry;" it allows artisans to sell their goods as well as allowing consumers to buy them. While there are those who object to the sales of t-shirts and sunglasses, we do not believe that 1) it is right to stop all vending because some people don't like some of the products, and 2) it is right to castigate those who bring supply to demand's level. However, often the vending stalls are unsightly and we do believe that provisions should be made to keep them looking nice.

Guideline:

Vending should be allowed to continue in Venice along the Ocean Front Walk, and near the proposed Windward historical district as it is in keeping with both Venice's past and Venice's nature. Guidelines should be set up pertaining to the acceptable look of a vending stall.

California's Chumash Indians
 Lynne McCall and Rosalind Perry;
 John Daniel, Publisher, Santa Barbara,
 CA, 1986



Historical Building Heights Houses

THE CHUMASH Indians lived in round thatched houses. To make a house, they set willow poles into the ground in a circle. Then they bent the poles toward the center to form a dome shape and lashed them together at the top. They tied smaller saplings around this frame, parallel to the ground, to form a lattice structure. They made an arched doorway, sometimes using large whale rib bones for this purpose.

The outside covering or thatching of the house was bulrush (Spanish: *lule*) or cattails, which they gathered in marshy areas. Large bundles of thatching were draped over the horizontal cross-pieces and tied on to prevent the wind from blowing them off. By starting at the bottom and working toward the top, each layer fell over the one below like shingles, and kept the rain out. At the top of the house a hole was left open for ventilation. In rainy weather, it was covered with a skin.

Each house had a fire pit in the center for heat or for cooking in bad weather. In fine weather, cooking was done outside. Twined *lule* mats were used to cover the doorway in cold weather, or to show that the owners of the house were not at home. In some areas people slept on platforms raised above the floor and covered with mats. The interiors of the houses were sometimes partitioned into rooms by hanging mats.

Single-family homes were usually twelve to twenty feet in diameter, depending on the size of the family. The chief's house was much larger, up to thirty-five feet across. An extended family, including relatives of different generations, shared living quarters.

Diameter
 4.25 inches Height
 3 inches (in drawing)

12 feet
 20 feet
 35 feet

8.47 feet
 14.12 feet
 24.71 feet for elected officials

chief's house:

N. Kent
 10-3-88

V

HISTORICAL
PRESERVATION

NORTH BEACH B/C

Buildings/Sites for Historic Designation - Preliminary List 9/6/88

Breakwater - 1905
Brick Street - 18th St.
Windward Colonnades
Waldorf Hotel - 1913 - exclusive hotel that housed many silent film stars
St. Charles Hotel - 1905 - (St. Marks Annex)
Sidewalk Cafe - 1915
Windward Apts. - 1906
80 Windward - 1905 - housed the Venice First National Bank
Vine covered brick bldg. on Westminster
52 Windward - speak easy during prohibition - underground tunnel
64-72 Market St. - 1913-14 - part of an arcaded business street

[INVENTORY LIST PRESENTED BY BETSY GOLDMAN, PRESIDENT - VENICE HISTORICAL SOCIETY, TO GROUP B/C, L.I.P. WORKSHOP, on 9-6-88. /lk]

HISTORICAL PRESERVATION

GOAL:

To preserve, protect and retain the historical and architectural character of Venice from demolition or inconsistent remodeling and to arrive at a series of historical preservation guidelines and a list of buildings, streets, trees, etc., that shall be protected from demolition by the City of Los Angeles.

COMMENTARY:

Venice has a unique array of historical architectural diversity, ranging from small well designed and crafted wood sided beach bungalows on the narrow intimate walk streets, to arcaded palazzos along wide and formal Windward Avenue.

Although there may be only a couple dozen buildings which merit the title of Historical Monument, there is a quality and ambiance of the combined buildings, streets and landscape which as a whole are most certainly worthy of historical preservation and protection.

Buildings are being torn down in Venice without the community's knowledge. We are seeing buildings that represent the type of architecture we are trying so hard to emulate, if not at least compliment. There needs to be some sort of immediate measure that can be taken to avoid more of "Old Venice" being destroyed.

IMPLEMENTATION:

Any building, street, or tree constructed or planted prior to 1940, should be protected from demolition or major remodeling by being brought before the community in the same manner as a request for a Zoning variance. This guideline is absolutely necessary to give the community time to protect and retain any historically important aspect of Venice from being destroyed or significantly altered.

A list of significant buildings, streets, trees, etc., should be compiled through a series of Historical Preservation Workshops which the City should organize in cooperation with the Venice Historical Society and the community of Venice.

Submitted By:

Lewin Wertheimer
26 October 1988